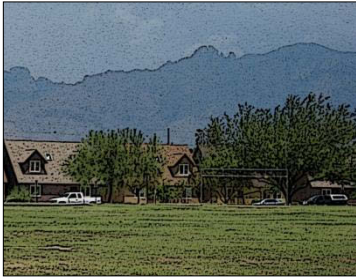


Grant Road District Zoning Ordinance



January 11, 2013

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1. Introduction and Purpose

1. INTRODUCTION AND PURPOSE

1.1 Background

On XXX X, 20XX, in accordance with Section 2.8.13 (Urban Overlay District Zone) of the Land Use Code (LUC), the City of Tucson's Mayor and Council initiated a rezoning to an urban overlay district for a geographic area focused on five miles of Grant Road from Oracle Road and Swan Road.

An urban overlay district allows for a zoning option that encourages walkable mixed use development that achieves the goals the community along Grant Road established through the Grant Road Improvement Plan planning effort. The urban overlay district (UOD) known as the Grant Road District (GRD) generally includes parcels along Grant Road between 15th Avenue in the West and Arcadia Avenue in the east, with the addition of some parcels along major cross streets. Figure 1 contains a map of the Grant Road District.

The GRD is a result of the Grant Road Improvement Plan, which comprehensively addresses the corridor of Grant Road between Oracle and Swan through both a new street design and the planning of the places along it. It became apparent to the City of Tucson and its transportation department (TDOT) that widening and improving Grant Road was more than a roadway project because of the economic development and community character opportunities that can result from creating a more multi-modal street, as well as the possibilities for providing land use and other related policies that allow for on-going use and improvement to parcels affected by right-of-way widening. The Grant Road Improvement Plan Vision Statement and Guiding Principles also indicate the need for places that support and complement the increased transportation options and walkability along Grant Road.

The UOD is the appropriate zoning option for the Grant Road project for a number of reasons. The Grant Road District is meant to implement the shared vision of the Grant Road corridor created through the Grant Road Improvement Plan's Community Character & Vitality planning and public involvement process. The shared vision for Grant Road was created through a multi-year process with input from a Citizens Task Force, corridor stakeholders (i.e., property and business owners, and adjacent residents), and the public at large, and resulted in a Grant Road Vision Statement & Guiding Principles, Planning Objectives and the Vision Concepts for Centers and Segments, which are included below. Overall, the Grant Road District aims to create a more walkable, vibrant, economically vital, and environmentally sustainable corridor that also protects the existing largely single-family residential neighborhoods adjacent to the mixed use areas along Grant Road.

The Grant Road District uses the Land Use Groups and Land Use Classes as defined in the Tucson LUC, dated April 4, 2011. See Article VI. Definitions. Division 3. Land Use Groups of the Tucson LUC for definitions. The uses allowed within the Grant Road District's zoning categories are described in the standards for each specific category, see GRD Section 3.

The GRD zoning option allows a property owner to choose to develop property with new uses and flexible development requirements in exchange for achieving a design of buildings and sites that meet the community's vision and guiding principles for the character of development along Grant Road. A property owner who has chosen the GRD zoning option cannot revert to the underlying zoning standards without applying for a modification of zoning requirements or pursuing a separate rezoning. The overlay district does not preclude a property owner from pursuing a rezoning for his property.

The purpose of GRD is consistent with applicable plans, including the Tucson General Plan and Area and Neighborhood Plans, in that it will:

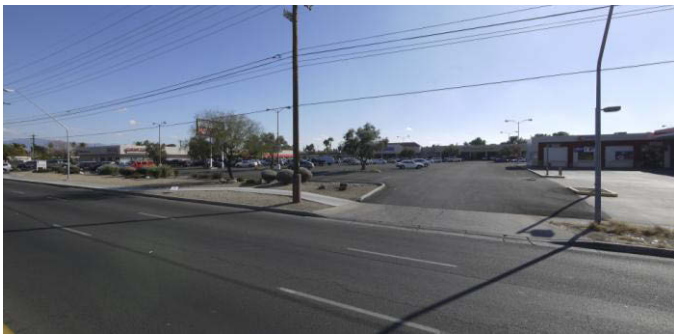
- balance residential, commercial and industrial uses along the corridor to meet the economic revitalization and community character goals for Grant Road;
- guide future development while protecting and improving existing neighborhoods;
- recognize distinct neighborhoods along Grant Road from Oracle to Swan;



1. Introduction and Purpose

- protect historic architectural styles along Grant Road and insure compatible development adjacent to residential areas;
- guide new development in harmony with community-wide planning objectives;
- recognize that office, commercial and high-density residential are primarily located along major streets; and
- identify appropriate locations for new development.

The GRD provides a range of benefits to the community such as those described in Vision Statement and Guiding Principles, Planning Objectives, and the Vision Concepts for Centers and Segments, which are included below.



Existing Development



After Redevelopment



Existing Development



After Redevelopment

Figure 1: Photosimulations of two locations along Grant Road illustrate the purpose of the Grant Road District. The first set of existing and after redevelopment conditions shows how an auto-oriented shopping center could transform into a walkable and bikeable mixed-use place with new parks, retail and office uses, as well as residences. The second set shows how an existing office building and retail building could transform into a set of new townhomes and residential flats and a mixed use retail and office building that create a more walkable environment along Grant Road as well as between new services on Grant Road and the existing single-family neighborhood beyond.

1. Introduction and Purpose

1.1.1 Grant Road Improvement Project Vision and Guiding Principles

The Vision Statement and Guiding Principles for the Grant Road Improvement Plan were created based on extensive public input, technical data, and the Grant Road citizens Task Force's understanding of the project. It was finalized and endorsed at the February 19th, 2008 Task Force meeting.

Vision Statement

The improvements resulting from the Grant Road Improvement Plan will enhance safety and balance mobility and accessibility for all users including motorists, pedestrians, bicyclists, transit riders, and those with disabilities. The Plan will direct the widening of Grant Road to six lanes, also improving its function as an urban arterial street within Tucson's network of streets. The Plan will balance the needs of those traveling through the area on Grant Road with those who live, work, and shop along Grant Road.

The community values the scale, character, and diversity of the neighborhoods and businesses along Grant Road and the Plan will reflect these values. The Plan will recognize the diversity of residents and independent businesses along Grant Road, and will help them to revitalize the places in which they live and work.

The Plan will strive to improve the visual character and quality of Grant Road and the land uses along it, and it will define Grant Road as a unique and vital place that ultimately enhances the community and region as a whole.

The Grant Road planning process and its implementation will balance a long range vision with the definition of improvements and programs that fit within the financial resources identified for this project. It will be forward-thinking in its design, consider likely future trends and work to effect positive change to the environment and public health. It will set high standards for community involvement while providing an inspirational model for future Tucson roadway enhancements.

Guiding Principles

1. Mobility and Access

Mobility and Access mean moving along and connecting with uses on Grant Road — both for people living and working nearby and those passing through; both freeing up motion (mobility) and getting to specific destinations (access). Through mobility and access, the Plan will work toward sustainable transportation both for the local community and the Tucson region by doing the following:

1.1 Balance the transportation needs of those traveling locally with those passing through Grant Road by:

- improving Grant Road's role in Tucson's street network and its role for neighbors;
- recognizing all populations using Grant Road; and by
- increasing the efficiency of traffic flow along Grant Road while designing Grant Road and the context along it to encourage drivers to travel at safe speeds.

1.2 Improve mobility and safety for all those traveling along and across Grant Road, including pedestrians, bicyclists, transit riders, and those with disabilities, by:

- recognizing that Grant Road is not the exclusive domain of cars;
- improving access for all modes of travel to neighborhoods, commercial districts, schools (including the University and community colleges), and other destinations;
- improving the physical conditions of the roadway, and the pedestrian and bicycle environments along and crossing Grant Road and on connecting streets;



1. Introduction and Purpose

- improving transit stops and access to them as well as considering the land uses around them; and by
- considering land use and other needs of transit, bicycle, and walking dependent populations.

1.3 Balance mobility along and across Grant Road with access to businesses, residences, and other destinations along and nearby Grant Road by:

- providing safe vehicular access to properties on Grant Road;
- facilitating regional access to businesses (including deliveries) that avoids cut-through traffic impacts to adjacent neighborhoods; and,
- improving neighborhood access to businesses and other destinations by providing quality connections for all users.

1.4 Ensure that roadway improvements support and enhance the community's values regarding the character, vitality, aesthetics, and environment of Grant Road by:

- Recognizing that different sections of Grant Road may require a different balance of mobility and access

1.5 Provide the potential for future transit options, such as bus rapid transit, modern streetcar, light rail, or other high-capacity transit in the design of Grant Road improvements with the goal of minimizing future costs for construction and right-of-way acquisition.

2. Character and Vitality

Character and Vitality mean the health of the places surrounding Grant Road — neighborhoods and businesses, public space and activity, and private investment. Character and Vitality define the overarching goals for aspects of the study area such as housing, neighborhoods, employment, and public space. Through character and vitality, the Plan will work to enhance, in a fair manner, the economic and social environment of neighborhoods and districts by doing the following:

2.1 Preserve and enhance the scale and character of existing residential neighborhoods by providing appropriate transitions and buffering from Grant Road and the uses fronting onto it to the neighborhoods behind.

2.2 Support opportunities for a range of options for housing tenancy and housing type which serve and expand upon the diversity of residents who live along and nearby Grant Road.

2.3 Support the viability of small, local, and independent businesses.

2.4 Preserve and enhance opportunities for a range of employment along Grant Road, including restaurants, retail, manufacturing, construction, repair, service, professional office and medical jobs.

2.5 Create a cohesive public realm that adds new public spaces to existing parks, plazas, schools and other community gathering places; forming an accessible network that supports and is supported by the design and function of Grant Road, and the neighborhoods and businesses along it.

2.6 Build on the attraction and strengths of community and social service organizations to revitalize districts and enhance the public realm with activity along Grant Road.

1. Introduction and Purpose

- 2.7 **Develop districts with multiple uses and shared parking** that will be destinations for neighborhood residents as well as people from the region at large.
- 2.8 **Recognize the differences in demographics, environment, scale, neighborhoods, business types, and other aspects of character;** and use them to reinforce the identities of Grant Road's Community Character Segments.
- 2.9 **Work to create safer environments that discourage crime** and increase personal safety.
- 2.10 **Support and build upon ethnic diversity** in relation to the social and economic vitality in the Grant Road Study Area.
- 2.11 **Encourage private investment** that revitalizes opportunity sites along Grant Road.

3. Aesthetics and Environment

Aesthetics and Environment build upon the principles set out in Character and Vitality by focusing on the details of key issues such as climate, utilities, views and the watershed. Through aesthetics and environment, the Plan will work toward human and ecological sustainability of Grant Road and the neighborhoods and districts along it to the benefit of those in the Study Area and the greater Tucson region by doing the following:

- 3.1 **Create an aesthetically pleasing, comfortable, inviting environment,** both in the street right-of-way and in adjacent public spaces, that is framed by the buildings and landscapes that front Grant Road.
- 3.2 **Enhance the identities** of Grant Road's Community Character Segments through the creation of business clusters, streetscape design, and other elements.
- 3.3 **Capitalize on Grant Road's natural environment and regional scenery** through climate adaptation, utilization of desert plants (especially those native to the Tucson basin), topography, key views and the integration of aesthetic and environmental design.
- 3.4 **Capitalize on Tucson's culture,** through urban form, architectural styles, public art, and other elements.
- 3.5 **Mitigate utility issues** including overhead wires, to the extent financially feasible.
- 3.6 **Mitigate watershed issues** such as flooding, stormwater runoff, ecological health and water harvesting in a holistic manner.
- 3.7 **Mitigate noise impacts** of traffic on Grant Road utilizing a range of techniques that are appropriate to the surrounding context.



1. Introduction and Purpose

4. Vision and Implementation

Vision and Implementation mean making the vision for Grant Road’s future a reality. Through vision and implementation, in a fair manner, the Plan will work incrementally towards long-term sustainability in transportation, economy, livability and ecology by doing the following:

- 4.1 **Define a long-range vision as well as priorities** that can be achieved within the budget and timing of the Grant Road Improvement Plan.
- 4.2 **Define cost-efficient and effective phases** for successful implementation.
- 4.3 **Protect the viability of businesses during construction** by maintaining their visibility, their parking, and access to them.
- 4.4 **Provide information and technical assistance to residential and business property owners** directly impacted by the Grant Road Plan.
- 4.5 **Define the improvements so that the vision can be achieved incrementally** with both the RTA funding base and additional public and private funding to enhance the improvements.
 - Identify and give priority to the implementation of those improvements that provide the most benefit and that address those issues that are a priority concern to the public.
 - Identify and pursue additional sources of funding early in the process to ensure that the desired improvements can be implemented.
- 4.6 **Rely on policies and programs** in addition to physical improvements in achieving the vision.
- 4.7 **Coordinate new development and revitalization with new and existing amenities and multimodal infrastructure.**

1.1.2 Grant Road Improvement Plan Community Character and Vitality Planning Objectives

The following objectives were developed during the Grant Road Improvement Planning process that has resulted in the GRD zoning. The objectives are the result of themes identified throughout the CC&V workshops, the design of the Grant Road street improvements, examination of existing city policy, and input from the Grant Road citizens Task Force, who endorsed them on November 15, 2010.

Encourage Viable and Compatible Land Uses

The Plan guides development toward land uses that are economically viable and compatible with the future vision for Grant Road’s Centers and Segments, as well as with the adjacent mainly single family neighborhoods. Current policy allows a wide range of land uses in most parts of the Grant Road project area, but lacks sufficient direction on how best to relate these different land uses to one another and to create the types of places desired by the public. The Plan guides how clustering complementary land uses and buffering between dissimilar land uses can help to create better places along Grant Road and support neighborhood goals for residential quality of life.

1. Introduction and Purpose

Create Context-Appropriate Building Heights and Massing

Building heights and massing in Grant Road's centers and segments must balance between respecting the largely low-rise Tucson built landscape and selective allowance for taller buildings. Taller buildings can accommodate the intensity needed to support vibrant street activity along Grant Road and the desired retail and commercial uses to serve both neighborhoods and the broader region. Public input from the Grant Road workshops has expressed both concern about the impacts of taller buildings and the acknowledgement of the benefits of taller buildings along major streets such as Grant Road. Well-designed building massing is essential to make taller buildings work in the context of Grant Road, where mountain views are valued by the community and historic architectural styles are predominantly one story.

Support Small and Independent Businesses

Much of Grant Road's current character is defined by the small, independent businesses along it. The public has indicated that it values these businesses, which include restaurants, building materials stores, antiques shops, among many others.

However, the widening of Grant Road and future revitalization along the street pose challenges for these small businesses, many of which rely on visible parking and inexpensive rents in older buildings. The widening will remove some of this front parking, and the Plan has developed strategies to replace parking while also supporting a walkable street environment along Grant Road to help businesses in the long term.

The Plan will also develop implementation strategies to help business owners stay in their buildings or have choices for space in new development, as well as center- or segment-wide strategies to encourage complementary clusters of businesses.

Protect and Enhance Neighborhoods

The existing mismatch between the auto-oriented nature of Grant Road and the quieter residential character of most adjacent neighborhoods has led to much discussion about protecting neighborhoods from the negative aspects of Grant Road; many policies of the area and neighborhood plans address this issue. The CC&V Plan guides the transition of building massing to the smaller scale of neighborhoods, and buffering with landscaped setbacks.

Provide, Improve, and Connect Public Spaces

The existing mismatch between the auto-oriented nature of Grant Road and the quieter residential character of most adjacent neighborhoods has led to much discussion about protecting neighborhoods from the negative aspects of Grant Road; many policies of the area and neighborhood plans address this issue. The CC&V Plan guides the transition of building massing to the smaller scale of neighborhoods, and buffering with landscaped setbacks.

Grant Road, and many of the neighborhoods along it, lack public spaces such as parks and plazas. These public spaces can become the community hubs that help generate the vibrancy along streets or neighborhoods, and can be a key component of an area's identity. The few public spaces along and near Grant Road, such as the "Triangle" park near Campbell Avenue and Mansfield Park, lack sufficient connections to both Grant Road and the surrounding areas, and therefore they are not the community assets that they should be. The community has expressed the desire to augment existing public spaces and add new public spaces, especially parks. The Grant Road street improvements will create a more walkable environment which will enhance and connect existing open spaces and, along with the CC&V Plan, provide the opportunity for more public spaces. It is critical that these public spaces integrate with their surroundings in order to maximize their safety and value.



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Balance Needs of Regional and Local Serving Businesses

The successful revitalization of Grant Road will rely on investment from the private sector. The CC&V Plan catalyzes the flow of capital into economic activity along Grant Road in a way that balances the needs of the greater Tucson region and surrounding neighborhoods. Many of the conflicting desires for the future environment along Grant Road reflect the differing needs of these neighborhood and region-wide users. The design for Grant Road’s improvements has addressed these types of conflicts in the roadway, but they are also present in the development fronting on Grant Road, such as in the mix of businesses that cater to different customer bases, and how people use different transportation modes to access those businesses. The CC&V Plan encourages patterns of development where these uses can be mutually supportive.

Contribute to Street Activity and Safety

The use of a context-sensitive design approach for Grant Road’s roadway and streetscape improvements will result in a more pedestrian and transit-supportive environment. The CC&V Plan sets policies and standards to create the pedestrian-supportive context that will take best advantage of Grant Road’s redesign.

Design concepts that the CC&V Plan will support include: providing “eyes on the street” by orienting buildings so that their entries and windows are in proximity to Grant Road; encouraging outdoor seating for restaurants and cafes and outdoor display for retail and service establishments in support of pedestrian activity; and providing on-site public gathering places with shade and shelter from the weather.

Provide Adequate and Well-Designed Parking

Parking is important for businesses along Grant Road, but the location of most parking spaces between the street and the buildings is an important factor that prevents Grant Road from being a walkable street. The improvements to Grant Road will impact the parking supply of many businesses that have spaces in front of their buildings, and the CC&V Plan contains strategies to replace this parking through techniques such as sharing parking with adjacent businesses, site reconfiguration, and district parking. The planning effort is also exploring making changes to city requirements for parking. For reconfigured sites and new development, parking can take forms and include design details that help support Grant Road objectives for walkability, vitality, and sustainability.

Increase Multi-Modal Access to Places along Grant Road

The Grant Road street improvements provide solutions for many of the access issues that have challenged motorists, bicyclists, pedestrians and transit riders along Grant Road. However, many of the properties fronting Grant Road also pose challenges to multi-modal access, in particular larger parcels that typically contain shopping centers.

The CC&V Plan provides policies and standards to improve pedestrian circulation within these larger properties, particularly helping pedestrians navigate through their large surface parking areas and making pedestrians feel more comfortable. The CC&V Plan will also provide measures for connection of the surrounding street network to Grant Road while also reducing the impacts of through traffic in adjacent neighborhoods. As larger sites are revitalized with new development, they can be places to accomplish these circulation network improvements.

Respect Important Views

During Grant Road workshops, views of the Catalina Mountains have been described as a scenic resource that should be preserved and enhanced. The Catalina Mountains provide a backdrop to the north side of Grant Road along the length of the corridor, though substantial views of the mountains occur only occasionally down cross streets and between buildings; much of the existing development on Grant Road, through low-rise 1 and 2-story buildings, effectively blocks views of the mountains. Almost none of the development on Grant Road frames or otherwise enhances these views. The CC&V Plan guides

1. Introduction and Purpose

massing and setback of buildings and placement of unbuilt space to frame quality views of the Catalina Mountains in balance with other Plan goals.

Complement the Tucson Environment

A successful and sustainable Grant Road will integrate with the natural Sonoran Desert environment and climate of the Tucson Basin. The development along Grant Road today has paved over many of the washes and natural features that define this environment, but new development along Grant Road can incorporate aspects of the natural environment such as native plants, fauna, mountain views, and the watershed, as well as of the climate with ample sunlight, solar heat, and wind. New development can also use resource-efficient techniques to mitigate the summer heat such as shading and buildings with courtyards, and the design of major rainwater harvesting features as shaded and usable “oases” along Grant Road. The CC&V Plan includes policies and guidance to increase sustainability.

Strengthen Community Identity

Participants in the Grant Road public workshops emphasized the desire to use Grant Road to communicate who they are as communities. Grant Road currently poses challenges to this goal, but each of the communities along Grant Road contains unique history and demographics that can be brought to Grant Road and contribute to its revitalization. This objective also means preserving and enhancing particularly valued businesses and institutions. Community members have expressed interest in drawing from various local traditions. The Public Art Master Plan provides ideas on how to incorporate history, ecology and other local aspects into both the street improvements and the built environment along Grant Road.

The Plan’s Centers and Segments are defined by concepts and standards that are scaled and characterized for their location along Grant Road.

Evoke Tucson’s History and Culture

Tucson’s history and culture makes its imprint throughout the Grant Road corridor, whether in the motel architecture of the Oracle area, the carts serving Sonoran street food, the city’s specialty home furnishings businesses, or in the eclectic front yards of properties along Grant Road. The corridor can evoke more from the area’s rich history and culture, at a variety of scales: Tucson architectural elements such as motorcourts, courtyards, and walls provide the time-tested development patterns that can be expressed through a variety of modern styles and uses.

Integrate Watershed Management in Site Design

Rainwater creates both an environmental benefit and a liability in the Grant Road corridor. Rainwater “runoff” that is often conveyed into storm drains largely results from the impervious surfaces created by buildings, roads and parking lots. This water is exposed to the introduction of pollutants and can make management of stormwater more challenging. At the same time, rainwater presents opportunities for irrigation. In recent years, the City of Tucson has developed policies that encourage and mandate water “harvesting,” or management of rainwater onsite. The new Grant Road likewise incorporates water harvesting into its design. The CC&V Plan also supports water quality and reuse of rainwater in the Grant Road corridor in a number of ways, including guidelines for permeability of parking areas, well-designed onsite infiltration and retention, incorporation of water harvesting into open space areas, and education about the watershed through public art.

1.1.3 Grant Road Center and Segment Vision Concepts

The following Center and Segment Vision Concepts were developed during the Grant Road Improvement Planning process that has resulted in the GRD zoning. The vision concepts are the result of public input received through a series of Community



1. Introduction and Purpose

Character and Vitality (CC&V) workshops, the design of the Grant Road street improvements, examination of existing city policy, and input from the Grant Road Citizens Task Force. The Vision Concepts were endorsed by the Task Force on November 15, 2010.

Oracle Center

Keep focus of center with some enhancements

The Oracle Center will:

- maintain a focus on regional automobile sales and service;
- evolve into a more diverse, walkable business district with the addition of neighborhood-serving businesses and residences;
- balance the regional access and visibility of auto businesses with a walkable environment along Grant Road and along side streets;
- focus intensity at the intersection of Grant and Oracle, with taller buildings carefully placed and designed to frame views of the Catalina Mountains;
- surround this intensity with less-intensive development that incorporates the mixed use commercial/industrial/residential character of the area into well-designed development sites;
- incorporate the interpretations of historic roadside motorcourt motel building form, architectural style, and signage of the Oracle corridor; and,
- provide a gateway to the new Grant Road corridor with a public art piece at the intersection with Oracle to enhance the sense of the gateway.

Stone Center

Keep focus of center with some enhancements

The Stone Center will:

- continue to contain a mix of businesses, including retail, restaurants, and motor vehicle services;
- develop a stronger identity and a more walkable environment;
- recognize that the Grant-Stone shopping center property is a key site which, in the short term, can incorporate a more walkable environment into a redesigned parking lot, and in the long term could be the site of more intensive development that could be mixed use, adding office and entertainment uses, and possibly residences;
- focus intensity at the intersection of Grant and Stone, with taller buildings carefully placed and designed to frame views of the Catalina Mountains;
- incorporate the interpretations of historic roadside motorcourt building forms that can accommodate retail, office, light industrial and residential land uses; and,
- serve as a gateway along Stone as one travels south toward downtown.

1. Introduction and Purpose

Fontana Center

Refocus auto-oriented commercial strips into neighborhood center

The Fontana Center will:

- transform into a safe neighborhood center that provides services to the surrounding areas, including the day-to-day goods and services residents need such as convenience groceries, restaurants, and laundry, as well as community amenities and gathering places such as a community center, or a public plaza or market;
- provide community space for neighborhood youths such as recreational, educational, or health-related uses that can take advantage of the adjacency to Mansfield Park;
- contain new multi-family homes and townhomes whose residents will help to activate the center;
- focus on the block between Grant Road and Mansfield Park, which will contain a medium-intensity mix of uses with buildings fronting onto Grant Road and Sahuaro Avenue, shared parking in the middle of the block, and medium building heights;
- provide comfortable and safe pedestrian access between Grant Road and Mansfield Park, including a route through the block; and,
- revitalize the large lot at the southwest corner of Grant and 6th Avenue which could accommodate carefully designed taller, mixed use buildings to build the intensity that would help support a neighborhood center.

1st Avenue Center

Revitalize shopping centers into mixed use centers

The 1st Avenue Center will:

- evolve into a place that serves neighbors, students, and others from around Tucson with a mix of large anchor stores and smaller businesses, with the potential to integrate professional offices, residences and open spaces;
- in the short term, improve pedestrian circulation in and access to the two major shopping centers on the northwest and northeast corners;
- in the long term, revitalize these shopping centers to incorporate a broader mix of uses, including professional office and residences in buildings fronting Grant Road, and improved circulation with new pedestrian-supportive streets within these large sites; and,
- accommodate on the northwest and northeast corners well-designed, taller buildings.

Campbell Center

Revitalize to capitalize on existing focus of center

The Campbell Center will:

- continue to develop into a vibrant gathering, shopping and dining destination for residents of surrounding neighborhoods, those affiliated with the University, and people from throughout the city;
- maintain and enhance land uses that are valued by the community, such as public spaces and unique retail, coffee, and dining businesses;
- incorporate residences as well as an educational and civic/entertainment facility that could serve residents and the University's faculty, staff and students while adding vitality to the center;



1. Introduction and Purpose

- improve and expand the center’s existing public spaces such as the northwest corner “Triangle” park and the linear plaza along the west side of Campbell Avenue to the south of Grant Road, each leveraging the Grant Road streetscape improvements to tie the open spaces to surrounding uses;
- recognize the block at the center’s southeast corner as an important opportunity site that offers a chance to accommodate a larger mixed-use project that could include residences, retail, an educational and civic/entertainment facility, as well as structured parking;
- blend new development at the southeast corner into the scale and character of the neighborhood by stepping building heights down along Edison Street and Norris Avenue and limiting land uses along these frontages to residences or an enhancement of the landscape buffer to the existing parking garage, should it remain;
- revitalize and intensify the blocks on the northeast corner; and,
- allow the center to benefit from a district-wide shared-parking approach, with opportunities that could include reusing the existing parking structure on the southeast corner, constructing a new parking structure, and potential for surface lots on the property located on the southeast side of Grant Road and Norris Avenue.

Tucson Center

Keep focus of center with some enhancements

The Tucson Center will:

- continue to support a mix of building supply stores, restaurants and other retail and service uses;
- focus intensity at the corner properties at the intersection of Tucson Boulevard and Grant Road with well-designed taller buildings, which will incorporate residences, retail, offices, or a mix of these uses to create a better pedestrian environment at the intersection;
- balance the need for auto access to businesses with a better pedestrian environment that benefits from areas in front of businesses that are no longer available for parking taking on more engaging uses such as seating, display, and landscaping; and,
- benefit from new district parking lots that will provide more flexibility for the development of individual parcels by reducing the need for on-site parking.

Country Club Center

Revitalize to capitalize on existing focus of center

The Country Club Center will:

- grow around two different but compatible clusters of activity: Doolen School, and the recreational and institutional uses that are adjacent to it; and the antiques district around the intersection of Grant and Country Club roads;
- emphasize a walkable environment for both students and shoppers;
- include complementary land uses such as food services and public space;
- incorporate residences in this center which will increase vitality and safety in the area, and potential live-work units could support the expansion of the antiques cluster with complementary arts and crafts businesses; and,
- accommodate more intensity in the form of well-designed buildings at Grant and Country Club Roads, with heights stepping down to the surrounding neighborhoods.

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Alvernon Center

Revitalize shopping centers into mixed use centers

The Alvernon Center will:

- build on its current role as a community shopping and transit hub;
- support the high number of pedestrians that currently walk along and across Grant Road to access the high-levels of transit service, and destinations in and beyond this center;
- improve existing development, especially to the larger shopping centers, such as landscape buffers, landscaped and shaded pedestrian walkways, and infill buildings;
- provide a longer-term vision for the larger shopping center sites so that if reuse occurs, the anchor retail stores that serve a wide segment of the community could remain, but the character of the sites will transform into a more intensive and diverse mix of uses such as retail, offices, and residences accommodated in buildings that are accessed by a finer network of streets; and,
- recognize that the Tucson Botanical Gardens is an asset to the area and should be showcased on Grant Road, whether through an expansion to front directly onto Grant Road, an access way on Grant Road, the addition of complementary businesses serving visitors, or an enhancement of Grant Road landscaping that could be maintained by the Gardens through an agreement - any of which would help to increase the visibility of the Gardens while supporting the community-building goals of the Grant Road CC&V Plan.

Columbus Center

Refocus auto-oriented commercial strips into neighborhood center

The Columbus Center will:

- coalesce into a walkable neighborhood center that provides a shopping and gathering destination that is more convenient and neighborhood-focused than stores in the larger, more regional-serving or specialty shopping centers at Alvernon and Country Club Centers;
- capitalize on many varied small opportunities for transformation, such as underutilized parcels fronting the walkable environment created by the local access lane west of Columbus Boulevard on the north side of Grant Road, a rear alley to access new retail, office, residential or live-work development on remnant pieces of acquired parcels on the south side of Grant Road, and vacant parcels northeast of the intersection of Columbus Boulevard and Grant Road;
- transition the current auto-oriented land uses around this intersection to more intensive, pedestrian-supportive, mixed-use development;
- incorporate mixed-use development with retail space for independent stores with medical offices or residences above; and,
- focus neighborhood-supporting uses with the development of a small public open space that would bring positive activity to support a safer environment in this center.

Swan Center

Keep focus of center with some enhancements

The Swan Center will:

- continue to be focused around the shopping centers and major commercial land uses that, along with the Crossroads Festival shopping center, comprise a major retail and restaurant cluster in Midtown Tucson;



1. Introduction and Purpose

- improve the walking environment at the edges of these shopping centers and partial or complete redevelopment of the northwest or southeast corners of the Grant and Swan intersection could further improve walking with the addition of buildings fronting the roads; and,
- incorporate a broader mix of uses, including professional or medical offices, and restaurants that could further enliven the center.

Western Segment areas

A balanced and supportive mix of uses: residential, student housing, commercial, industrial

The Stone to Fontana Segment will:

- continue to contain a mix of commercial and industrial businesses;
- become a walkable transition between the more regional-serving Stone and Oracle Centers and the more neighborhood-serving Fontana Center;
- support the Grant Road sidewalk environment with improvements such as buffering to parking lots and loading areas, as well as use of alleys on the south side to shift auto access to properties away from their Grant Road frontage; and,
- incorporate the eclectic character and the employment and economic activity of the mixed commercial/industrial area to the south.

The Fontana to 1st Segment (north side) will:

- continue to contain a mix of commercial and auto businesses;
- provide a transition between the more regional-serving 1st Avenue Center and the more neighborhood-serving Fontana Center; and,
- support the Grant Road sidewalk environment with improvements such as buffers from parking lots.

The Fontana to 1st Segment (south side) will:

- continue to be defined largely by single family houses siding onto Grant Road, which, newly exposed to Grant Road, could be buffered by soundwalls and landscaping between Grant Road and the side yards of the homes.

Central Segment areas

Active and engaging neighborhood buffers

The areas in the Central Segment on either side of the Campbell Center will:

- support the historic single family residential character of the surrounding neighborhoods of Jefferson Park, Samos, Catalina Vista, Campbell-Grant, and Blenman Elm;
- provide an interesting, engaging frontage along Grant Road that encourages activity, safety, and walking;
- revitalize the remnant parcels that remain following the widening of Grant Road with new buildings compatible with surrounding neighborhoods, and land uses that largely reflect existing uses.
- design this new development to be more intense than the surrounding neighborhoods which will make new buildings buffers between Grant Road and the existing residences while providing comfortable environments for living, and where appropriate for working, through configurations such as walled courtyards or side access via pedestrian walks; and,

1. Introduction and Purpose

- integrate open spaces into new development, either as individual pocket parks or as one or more linear parks that may connect to the existing “Triangle” park near Campbell Avenue. These parks will be primarily fronted by development so occupants can take “ownership” of the parks and help fund their construction and maintenance.

Eastern Segment areas

Support business viability and enhance community character

The Eastern Segment districts will:

- continue to benefit from valued independent businesses;
- support these businesses through the mitigation of impacts to parking from the Grant Road widening, as well as through the strengthening of business district identities and industry clusters such as medical office and services, antiques, restaurants, and building supplies;
- provide district parking lots, which can be shared by customers of various businesses, can provide signage and education about businesses, and can create public space for events such as markets and festivals through periodic use of parking lots;
- diversify land uses to decrease the over-supply of retail and service space along this stretch of Grant Road with the addition of offices and residences, which can make the districts more viable and active;
- leverage the new Grant Road design to create a better pedestrian environment, which will take advantage of the direct frontage onto the sidewalk where continuous parking lots are no longer possible, and the redevelopment of space unusable for parking into attractive landscaping, dining, and display areas; and,
- incorporate new infill development that will increase vitality in the Eastern Segment districts and create a continuous and active building frontage along the sidewalk and stepping down of heights to and buffering adjacent residential neighborhoods.

1.2 Grant Road District Standards Approach

The Grant Road District’s approach differs from that of the current City of Tucson Land Use Code in a few key ways.

First, in contrast to the Tucson Land Use Code, the Grant Road District standards emphasize the physical form of development and the relationship of development to surrounding neighborhoods and other adjacent properties, streets, and public open spaces. While the Grant Road District provides standards related to allowed uses and has performance standards for some uses, it does not regulate use to the level of specificity that the LUC does in existing zoning districts. The GRD seeks to provide flexibility in specific land use while protecting surrounding uses from potential negative impacts, like noise and dust. Therefore, the GRD can more adequately address priorities for good urbanism and placemaking, such as allowing for mixing of uses and flexibility of use over time, and more tightly controlling the relationship between a building and the sidewalk, and a building and an adjacent single-family neighborhood. Because of this approach, the Grant Road District is more flexible with regard to land use, as appropriate to specific locations along Grant Road. All of these consequences are supported by the community’s objectives for the future character of businesses, residences, and open spaces along Grant Road, and a pattern that focuses the most intensive uses and development within mixed use and walkable centers.

Second, the Grant Road District is built around place types that treat major streets as “seams” rather than boundaries and “dividers”. Many current City of Tucson Area and Neighborhood Plans largely treat Grant Road and other Major Streets and Routes as edges of both plan areas and established neighborhoods, and as such are not able to holistically consider Grant Road and its major intersections as unified, important places unto themselves and in a complementary relationship with the adjacent neighborhoods. The Grant Road District, in contrast, is built of place types that center on Grant Road and its major intersections. The GRD is built of two such place types: Centers and Segments. A Center is an area around Grant Road’s intersections with other major roads that contains concentrations of a mix of activities. Centers vary in size, scale, and



1. Introduction and Purpose

diversity and are designed to be compatible with the residential neighborhoods around them. A Segment is an area that lines Grant Road between the Centers whose activity levels and buildings typically are smaller-scale. While some Segments contain auto-serving uses, they also support pedestrian and bicycle movement along Grant Road. Centers and Segments are the fundamental building block of the Grant Road Community Character and Vitality Plan and the Grant Road District; the distinctions among the GRD's zoning categories are largely based on assembling successful Centers and Segments.

2. Establishment and Administration of the GRD Standards

2. ESTABLISHMENT AND ADMINISTRATION OF THE GRANT ROAD DISTRICT STANDARDS

The Grant Road District is an optional Urban Overlay Zone (LUC Section 2.8.13) for owners of property within the zone boundaries. Property owners may process their development application using the existing zoning or the overlay zone, which is meant to achieve the vision for Grant Road as defined in the Area and Neighborhood Plans that include the Grant Road planning area and the future vision and planning objectives of the Grant Road Community Character & Vitality Plan. Once a property owner selects to develop under the Grant Road District, the applicable GRD category becomes the zoning designation for the property and the previous zoning designation is no longer applicable.

The Grant Road District zoning standards include applicable sections of the City of Tucson Land Use Code (Chapter 23 of the Tucson Code, dated July 1, 1995 with supplements through to No. 56, dated August 14, 2011) with specific sections and exceptions defined in the GRD zoning standards. The Grant Road District zoning standards also include applicable sections of the City of Tucson Development Standards (with updates through May 1, 2009) with specific sections and exceptions defined in the GRD zoning standards.

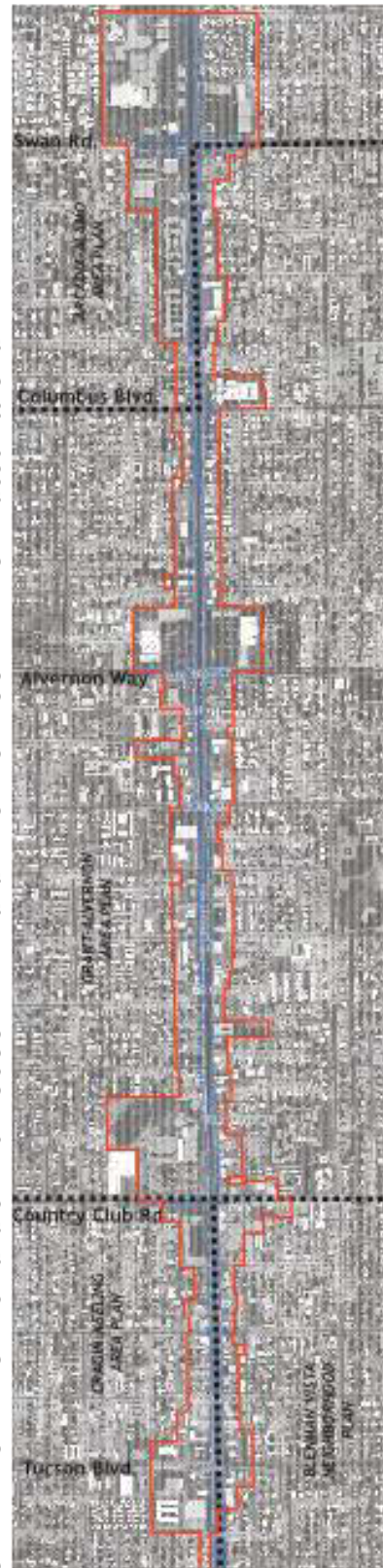
Parcels where the Grant Road District is an option are identified in the Grant Road District boundary map, see Figure 2.a.

2. Establishment and Administration of the GRD Standards

GRANT ROAD DISTRICT and CITY of TUCSON AREA and NEIGHBORHOOD PLANS - Oracle to Tucson



GRANT ROAD DISTRICT and CITY of TUCSON AREA and NEIGHBORHOOD PLANS - Tucson to Swan



- Potential Grant Road District (GRD) boundary
- - - Area or Neighborhood Plan boundary
- Road Alignment

Figure 2a: Grant Road District Boundary on map illustrating the current design of the Grant Road Improvement Project and the Area and Neighborhood Plan Boundaries.

2. Establishment and Administration of the GRD Standards

2.1 Establishment of the Grant Road District

- Upon adoption of the rezoning ordinance to urban overlay district by the Mayor and Council on _____, 201X **{insert adoption date}**, the Grant Road District is established.
- The Mayor and Council may amend or dissolve the district by ordinance under the Zoning Examiner Legislative Process, Sec 5.4.1, and Sec. 5.4.3.
- A property owner may request the zoning on his property be amended by submitting an application, paying applicable rezoning fees and proceeding through the Zoning Examiner Legislative Process, Sec 5.4.1, and Sec. 5.4.3.
- The Grant Road District's boundaries shall be identified on the City's of Tucson's Zoning Maps.
- A property owner using the GRD zoning option shall have her existing zoning designation prefaced by a U on the City of Tucson's Zoning Maps. An owner of property located in a Historic Preservation Zone that uses the GRD zoning option shall have the current zoning designation H prefaced by a U, i.e., HR-3 becomes U-HR-3.
- A property owner may elect to develop and use her property either under the existing underlying zoning district or under the Grant Road District zoning option; provided, however, that properties located in a Historic Preservation Zone which are developed or redeveloped under the Grant Road District zoning option are also required to comply with certain requirements of LUC Section 2.8.8 as provided in Section C-18 (Historic Preservation). Plans submitted under the Grant Road District zoning option shall comply with the regulations herein.
- A property owner using the GRD zoning option shall comply with all of the appropriate sections of the GRD zoning ordinance.
- A Grant Road District Development Package for development under the GRD cannot be used in conjunction with other waivers or modification provisions of the LUC and applicants cannot select the provisions of other overlay zones except as expressly provided in this GRD development document.
- The Grant Road District zoning option for a property shall be effective upon the issuance of a building permit for a project being developed or altered in accordance with its requirements. The owner of an existing development in the Grant Road District may elect to develop or use her property under the Grant Road District requirements by so stating in an application for a certificate of occupancy, and the election of the zoning option shall be effective upon the issuance of the certificate of occupancy.
- Unless otherwise specified in the GRD zoning document, the LUC and its supplemental documents shall apply. Where there is a conflict between the Land Use Code and its supplemental documents' requirements and the GRD's requirements, the GRD requirements shall control.
- Upon any reorganization or renumbering of the Land Use Code, Development Standards or Development Compliance Code, in connection with the adoption of a Unified Development Code or otherwise, the references thereto in this UOD shall refer to the pertinent provision of the reorganized or renumbered code. [Note that the references to the LUC in this draft of the GRD refer to the numbering and organization of the LUC dated April 4, 2011].



2. Establishment and Administration of the GRD Standards

2.2 Applicability of the Grant Road District Standards

2.2.1 Criteria for opting in to the GRD

A property owner might opt into the GRD for a variety of reasons. The most common reason is likely that the property owner would find himself or herself in a situation where the benefits of the GRD exceed those of his or her existing zoning, such as the range of uses allowed or the amount of development capacity allowed. Other examples of reasons that a property owner might opt in to the GRD include:

- New construction permits and/or Certificate of Occupancy;
- Adaptive reuse of existing buildings for wider use range; or,
- Participation in a district parking and/or shared parking scheme.

It should be noted that in cases where existing buildings, parking, or other elements of the site would be reused, they may need to be modified to meet the zoning and design standards of the GRD. (It is not the intent of the GRD to provide additional flexibility in satisfying the zoning and design standards for existing development beyond the requirements of the zoning. New development or reuse of existing development must fully comply with the requirements of the GRD. This is because the GRD is designed to balance the benefits to the property owner with protections for adjacent property owners, including neighborhoods. If a property owner cannot meet the GRD requirements they need to go through a standard zoning, variance, or other process based on their existing zoning.)

2.2.2 Historic Properties

Use of the GRD is intended to be compatible with historic preservation. Effective **XXXX**, any modification to “listed” or “eligible-to-be-listed” historic structure or structures that precedes, follows, or is part of a development (including alterations, additions, and full or partial demolitions) that, in the evaluation of the City of Tucson Historic Preservation Officer (COTHPO), does not meet the Secretary of Interior’s Standards for the Treatment of Historic Properties and would cause the historic building or structure to be de-listed from the National Register of Historic Places, or cause it to become ineligible for listing in the National Register, disqualifies the use of the GRD zoning option for the portion of the site that would have this detrimental effect on the historic building or structure. Any development proposing to use the GRD requires a written concurrence from the COTHPO that the National Register listed or eligible-to-be-listed properties will not be de-listed or made ineligible for listing. Appeals of the COTHPO decision by any interested party can be made to the State Historic Preservation Officer (SHPO). City of Tucson activities (including full or partial demolitions) associated with the construction of the Grant Road Improvement Project are excluded from this Historic Preservation standard.

Appeals of any decisions by PDSO relative to the GRD shall be made to the Board of Adjustments of the City of Tucson in accordance with its procedures.

2.3 Grant Road District Development Package

Development packages for projects submitting applications under the GRD or to elect into the GRD shall comply with the following:

- Development package submittals require approval by the City of Tucson (except where state law preempts local jurisdiction).

2. Establishment and Administration of the GRD Standards

- Applicants electing to develop under the GRD zoning must submit a development package in compliance with applicable GRD requirements.
- Except as provided herein, a development package for development under the GRD zoning shall be prepared in compliance with City of Tucson Development Standard No. 2-01.0.0 and in compliance with other requirements imposed by the GRD. The Planning and Development Services Director may require applications to provide sufficient drawings and information to demonstrate compliance with the GRD requirements. The title block required by Section 2.4 of Development Standard No. 2-01.0.0 shall include the statement “Development under Grant Road District zoning.” The development package submittal shall include the communication from the City’s Design Professional described in Section 2.5-Review and Approval Procedures of the GRD.
- In addition to the documentation required by Development Standard No. 2- 01 et seq.,
 - A submittal indicating how the project complies with GRD Frontage Type requirements. This submittal should be done in three steps and may include one or more site plans and other drawings such as elevations and/or sections:
 - **Step 1:** Indicate all of the *public frontage* in the project. *Public frontage* is defined in Section 5, Definitions. In the Public Yard, Courtyard, and/or Shared Motorcourt, the site plan shall also indicate how the proposal satisfies the requirements frontage onto the yard or court
 - **Step 2:** For each length of *public frontage*, indicate which frontage type is being applied. The applicant should confirm that that the application of each of these frontage types complies with the allowed frontage types listed for the project’s zoning category.
 - **Step 3:** For each length of *public frontage*, provide the necessary graphic information to demonstrate that the project satisfies the requirements of the frontage type being applied.
 - The development package should contain materials sufficient to evaluate whether the project meets the requirements of Section 4.5 Street Frontage and Building Placement Standards.
 - The Director may require an applicant to provide elevations sufficient to demonstrate compliance with Section 4.5.5 Frontage Types, Section 4.6 Building Height, Massing and Articulation Standards, and Section 3 Grant Road District Categories for the specific zoning category and center or segment location of the proposed project in regards to building height, massing, and articulation; and in particular upper floor setbacks.
 - The Director may require study illustrating compliance with the requirements of Section 4.6.3 – Privacy Considerations when Adjacent to Existing Residential Zoning which could include a site plan, sections, and/or elevations of the project property and adjacent existing residentially zoned property(ies) in order to facilitate review of the requirements of this section of the GRD.
- Applicants seeking a reduction of the minimum motor vehicle parking requirements contained in Section C-5 must provide a parking statement explaining the justification for the reduction and may be required by the Director to provide a Parking Impact Analysis prepared by a qualified, third-party consulting traffic engineer.
- Applicants seeking approval for a building more than 40 feet or 3-stories in height shall submit a visual analysis of the proposed development to review building heights and impacts on mountain views and solar access to adjacent properties.
- The Director will require that a Parking Impact Analysis and/or a Traffic Impact Analysis prepared by a qualified, third-party consulting traffic engineer be provided for projects seeking or utilizing:
 - Access to site from a side street and/or alley by more than 30 vehicles per day or when alley is used for loading access
 - Shared parking approval, an Individual Parking Plan is also required, see Section 4.7.1
 - District parking approval or use of existing district parking, see Section 4.7.1
 - Approval for new streets



2. Establishment and Administration of the GRD Standards

2.4 Pre-Application Process

Each Applicant shall meet with PDSD staff in a pre-application conference to verify the requirements for development in the Applicant's GRD category. The conference is intended to be an informal opportunity for the Applicant and staff to discuss the proposed project and review the proposals for important project features such as building location, site access, trash/recycling collection, on-site retention, the existence and proposed treatment of any Contributing Properties, and any adjacencies to existing residential neighborhoods and their local streets. Staff will provide direction as to how the project meets the purpose of the GRD and advise the Applicant of any additional submittal requirements (e.g., demolition review, traffic statement, parking plan, design review process).

The Applicant should bring information regarding the boundaries and location of their property(ies) they are applying for designation into and development under the GRD requirements, and any initial information they have developed regarding the project they will be submitting.

2.5 Review and Approval Procedures

The PDSD, and as appropriate with the additional review and comment by the City Design Professional, shall review Grant Road District Development Packages pursuant to the City of Tucson Development Compliance Code Sections 23A-31 (Zoning Compliance Review) and 23A-34 (Development Plan Review) except that a pre-application conference, see Section 2.4, is required.

2.5.1 Notification Requirements

Any development or use change that occurs for a property using the GRD shall include notification of property owners within 50 feet of the subject site, and a neighborhood meeting with notified property owners is encouraged, but not required. Projects that utilize a special exception or would otherwise be required to provide broader notification or require a neighborhood meeting shall comply with those requirements of the GRD and the LUC.

2.5.2 Review and Approval/Denial

2.5.2.1 Review per Design Standards

Design review for projects developed under the GRD zoning are to be done by PDSD and other appropriate city staff that report to the CDRC with the exception of items that are highlighted in the text of the GRD standards for review by the City's Design Professional (CDP); a checklist describing the sections of the standards that require staff or CDP review is in Appendix A of this document.

Proposed projects within the Jefferson Park NPZ

In addition to the standard review of submittal packages with the standards of the GRD, for GRD proposals within the Jefferson Park NPZ the CDP will perform a compatibility review based on Jefferson Park Design Manual based on LUC Sec. 2.8.11.

2. Establishment and Administration of the GRD Standards

2.5.2.2 Historic Preservation Review (if applicable)

In addition to (and prior to) review pursuant to other subsections of Section 2.5:

- New construction, development, redevelopment, additions and alterations of a Contributing Property outside of a Historic Preservation Zone shall be submitted (i) for review by the appropriate Historic Zone Advisory Board or the CDP, as applicable; and (ii) for review pursuant to LUC Section 2.8.8.5 (Historic Preservation Zone Development Review) applying the pertinent historic design review criteria.
- New construction, development, redevelopment, additions and alterations of property in a Historic Preservation Zone shall be submitted (i) to a neighborhood meeting pursuant to a notice mailed at the applicant's expense to all property owners (based on the last property assessment) in the Historic Preservation Zone; and (ii) for review by the appropriate Historic Zone Advisory Board; and (ii) for review pursuant to LUC Section 2.8.8.5 (Historic Preservation Zone Development Review) applying the pertinent historic design review criteria.

Requirements for All Construction or Improvements

- The requirements of LUC Sections 2.8.8.6.A (HPZ Development Criteria—Generally) (applying the development criteria indicated in this Section 2.5.3.3), 2.8.8.6.F (HPZ Development Criteria—Surface Texture), 2.8.8.6.M (HPZ Development Criteria—Signs), and 2.8.8.8 (HPZ—Maintenance) shall apply to (i) Contributing Property to a National Register District and (ii) property in a Historic Preservation Zone.

Additions or Alterations to Contributing Properties

- The requirements of LUC Sections 2.8.8.6.E (HPZ Development Criteria—Roof Types), 2.8.8.6.H (HPZ Development Criteria—Projections and Recessions), 2.8.8.6.I (HPZ Development Criteria—Details), 2.8.8.6.J (HPZ Development Criteria—Building Form), 2.8.8.6.K (HPZ Development Criteria—Rhythm), and items (1), (3) and (4) of Section 2.8.8.6.L (HPZ Development Criteria—Additional Review Criteria) shall apply to additions to or alterations of Contributing Properties.
- Unless an alteration of or addition to Contributing Property is approved by the Mayor and Council through a procedure conducted under Development Compliance Code Section 23A-62 (Mayor and Council Appeal Procedure), any alteration of or addition to Contributing Property shall be designed so as to retain its status as a Contributing Property.

Demolition

- Contributing Properties may not be demolished without compliance with LUC Sections 2.8.8.7 (Historic Preservation Zone--Demolition Review Required) and 2.8.8.9 (Historic Preservation Zone--Demolition of Historic Properties, Landmarks and Structures) (which, in Section 2.8.8.9.D, requires review by the appropriate Historic Zone Advisory Board and/or the Tucson-Pima County Historical Commission prior to approval by Mayor and Council).

Non-Historic, Non-Contributing Property

- Development of property that is neither a Contributing Property in a National Register District nor located in a Historic Preservation Zone but which is adjacent to a Contributing Property shall be designed to complement the architecture of the Contributing Property. Architecture that complements existing Contributing Properties shall be achieved through elements such as styles, colors, forms and materials, but is not intended to impose additional limitations on the Allowable Heights in the GRD, and by addressing privacy mitigation and building preservation through architectural elements such as building step-backs and setbacks, textures, materials, forms, and landscaping.



2. Establishment and Administration of the GRD Standards

Financial Incentives

- Owners of Contributing Properties may be eligible for various non-municipal tax incentives. Property owners are encouraged to contact the City of Tucson Historic Preservation Office for additional information.

GRD and HPZ

- Except as specifically provided in this Section 2.5.3.3, the provisions of LUC Section 2.8.8 (Historic Preservation Zone) shall not apply to property developed under the Grant Road District zoning.

2.5.2.3 Review Fees

Standard city review fees shall apply to projects submitted under the Grant Road District zoning.

2.6 Special Exception Land Uses and Development Criteria – Additional Review Process and Notification

Throughout the Grant Road District zoning standards there are special exceptions and allowances for specific land uses, sizes of projects, building heights, etc. that are Special Exceptions. These Special Exceptions require additional notification of surrounding stakeholders and additional review processes in order for a proposed project to be approved.

2.6.1 Grant Road Modified Limited Notice Procedure

Projects and procedures of Chapter 23A Development Code Compliance, Section 23A-40 shall apply with the following exceptions:

- Eligibility for limited notice procedures per the director's determination shall be based upon the following:
 - A minor change in the development criteria may not be requested through a GRD Limited Notice Procedure.
 - The proposed project utilizes primary or secondary vehicular and loading access to site from an alley or side street (excludes solid waste pickup) when adjacent to residential area.
 - Other proposed project conditions as noted in the design standards of the GRD.
- Pre-application conference is required per Section 2.4 of the GRD.
- Pre-application neighborhood contact per Section 23A-40.
- Applications shall conform with the requirements of the GRD.
- Notice of submittal of application per Section 23A-40.
- Public comment period shall be a period of five (5) days following the date on which notice is provided for submission of comments on the proposal to the development services department.
- The review process shall follow the procedures outlined in Section 2.5 of the GRD.
- Appeal to the board of adjustment. A party of record may submit an appeal of the decision to the board of adjustment in accordance with section 23A-61. A notice of intent to appeal must be received by the development services department within five (5) days after the notice of decision. The complete appeal materials must be filed within fifteen (15) days of the decision.

2. Establishment and Administration of the GRD Standards

2.6.2 PDS D Director approval through Limited Notice Procedure

Projects and procedures of Chapter 23A Development Code Compliance, Section 23A-40 shall apply with the following exceptions:

- Eligibility for limited notice procedures per the director's determination shall be based upon the following:
 - Decrease in parking provided below what is required
 - Use of shared parking, shared loading
 - Use of a Parking District when adjacent to residential area
 - Drive-throughs
 - Other proposed project conditions as noted in the design standards of the GRD.

2.6.3 Approval through Full Notice Procedure

Projects and procedures of Chapter 23A Development Code Compliance, Section 23A-50 shall apply with the following exceptions:

- Eligibility full notice procedures per the director's determination shall be based upon the following:
 - Projects generating traffic impacts per GRD Section 4.4.5(per Section 23A-51 –DSD full notice procedure)
 - Projects utilizing the 12' of additional building height within the GRD zoning categories' development standards(per section 23A-52 – Board of Adjustments full notice procedure)
 - Projects over 100,000 sq. ft. of leasable floor area(per section 23A-52 – Board of Adjustments full notice procedure)
 - Group dwellings when adjacent to R-1 zoned areas (per section 23A-52 – Board of Adjustments full notice procedure)
 - Projects proposing a change in GRD zoning category (per section 23A-53 – Zoning Examiner full notice procedure)
 - Other proposed project conditions as noted in the design standards of the GRD.
- Neighborhood meeting requirements per Section 23A-50 with the exception that the meeting shall occur at least five (5) and not more than sixty (60) days prior to the submittal of the application.
- Application shall conform with the requirements of the GRD zoning standards and administrative section. An application is accepted or rejected within five (5) days of the date of receipt unless the applicant consents to additional time. An application may, in the discretion of the director, be conditionally accepted.

2.7 Variances

Variances are reviewed through the Board of Adjustment Full Notice Procedure per LUC Sec. 5.3.3.

2.8 Modifications and Amendments to GRD

Amendments to the Grant Road District zoning may be necessary over time in order to respond to changing context, market or financial conditions or to respond to the unanticipated needs of new users. Minor or non-substantial changes (such as analogous uses, interior tax code boundary changes, minor adjustments to the standards that are consistent with the stated goals of the GRD) to the GRD must be submitted through PDS D staff and with staff recommendation, may be approved by the



2. Establishment and Administration of the GRD Standards

Development Services Director. Substantial changes that contradict the intent of this GRD shall require an amendment as per LUC UOD 2.8.13.9.

2.9 Implementation Monitoring and Reporting

GRD monitoring and results evaluation shall occur at intervals associated with each phase of the roadway construction, and at least at 5 year intervals following the completion of the Grant Road improvements. PDSD staff, with input from any City Design Professionals who have been involved with implementation of the GRD, shall review progress of redevelopment of sites, approvals process and applicability of standards, public input regarding development proposals and approved developments, etc. Staff reports shall be made to Planning Commission and to Mayor and Council at the intervals outlined above. Amendments based on monitoring results will follow the process as outlined in Section 2.8.

2.10 Rezoning

Property owner may choose to rezone to another zoning designation and out of the GRD through a full rezoning process under the Zoning Examiner Legislative Process at a public hearing for recommendation to the Mayor and Council per Sec. 5.4.1 and Sec. 5.4.3.

2.11 Traffic and Parking Mitigation Fees

If the City, as the result of traffic or parking impact studies or otherwise, and per the action of Mayor and Council elects to impose fees in connection with development using the Grant Road District zoning to mitigate the costs of traffic and parking infrastructure serving or related to activity and development within the Grant Road District, then applicants shall be required to pay such fees as a condition to the Director's approval of projects using the Grant Road District zoning.

2.12 How to use these Standards

- The Grant Road District supersedes portions of the City of Tucson Code Chapter 23 Land Use Code and Chapter 3 Sign Code, the City of Tucson Development Standards, and the City of Tucson/Pima County Outdoor Lighting Code.
- Applicants opting to use the GRD need to look at two sets of standards in this document:
 - A. The General Zoning Standards, which apply to all properties within the GRD boundary.
 - B. The standards for the specific zoning category to which the applicant's property has been assigned in Figure 3.a.
- The Grant Road District standards include definitions of terms that are unique to the GRD in Section 7 of this document. Terms that are defined are indicated by italicized text throughout the document.
- Standards that are meant to be reviewed by PDSD staff are indicated by a ➤ bullet, while standards meant to be reviewed by a design professional are indicated by a ❖ bullet. These special bullets are used throughout the GRD document and are summarized in Appendix A – PDSD and City Design Professional Checklist.

3. Grant Road District Categories

3. GRANT ROAD DISTRICT CATEGORIES

3.1 Purpose

In order to achieve the community character along Grant Road set out in the Vision Concepts for Centers and Segments, the Grant Road District has seven (7) zoning categories. These categories address the varying conditions within the GRD and reinforce the desired character for the Centers and Segments along Grant Road. These categories include four (4) Center categories and three (3) Segment categories. The standards for each of these categories are in addition to the standards in the General Standards section of the GRD.

3.2 Applicability

These categories are applicable to specific properties according to Figure 3.a below.

3.3 Center and Segment Category Standards

Each property in the Grant Road District has been assigned one of seven zoning categories, see Figure 3.a. The General Standards apply to all zoning categories, while category-specific standards apply only to properties in that category. The following pages specify the standards for each GRD Category.

3.3.1 Center Categories

Center Marketplace

This category directs reconfiguration of large shopping center sites into retail-centered mixed use places with improved multimodal access. It allows for phased transformation over time; allows intensity and creates walkability on new streets within sites; and requires high degrees of direct frontage onto Grant Road – whether for new “liner” buildings in existing sites or for newer larger buildings that may be part of a fully revitalized site.

Center Hub

This category creates active, mixed-use focal points at major intersections in centers with higher-intensity development that is sensitive to adjacent neighborhood context. This category includes key opportunity sites where active, mixed use community focal points are possible and desired by the community. This designation allows uses that reflect the particular character of specific centers, such as allowing auto sales and service in the western portions of Grant Road.

Center Neighborhood

This category coalesces development into medium-intensity neighborhood-serving gathering places that draw neighborhood residents to walk to and along the new Grant Road. The category allows for a flexibility of use and intensity of activity that will encourage substantial revitalization of properties in order to transform auto-oriented strips into walkable neighborhood centers.

Center General

This category provides a transition from the more intensive areas of centers to adjacent Segments. This category often applies to smaller or shallower lots. Its standards provide transitions to the level and type of activity in adjacent Segments. It is intended to help preserve current viable commercial uses on Grant Road while enhancing the pedestrian environment.



3. Grant Road District Categories

3.3.2 Segment Categories

Segment West

This category balances and supports a wide mix of uses with reinterpreted classic roadside architecture, the Motorcourt Frontage Type, that supports pedestrian activity along Grant Road. It mixes commercial, auto, industrial, retail, and residential uses. It balances the needs of regional-serving auto-based businesses and neighborhood-serving businesses.

Segment Central

This category creates active and engaging buffers between adjacent single-family neighborhoods and Grant Road. It allows for uses, heights, and massing that blend with the character of adjacent historic single-family residential neighborhoods while creating frontages onto Grant Road and adjacent streets that support the desired improvement to the environment for pedestrians.

Segment East

This category supports commercial viability and enhances community character. With retail and intensity of development focused in the Grant Road District's Centers, this category increases the viability and vitality of Grant Road by allowing for a diversity of land uses with the addition of more medium and low intensity office and residential uses. It also seeks to adapt properties to the walkable environment of the new Grant Road while allowing for enough auto parking to serve the types of uses that are likely to locate in this area.

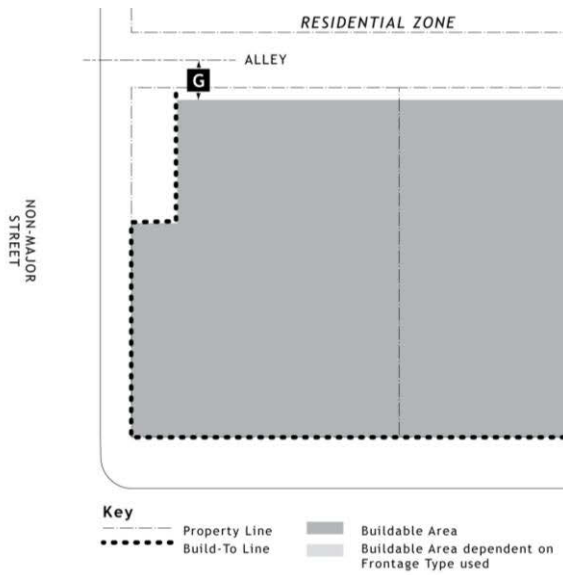
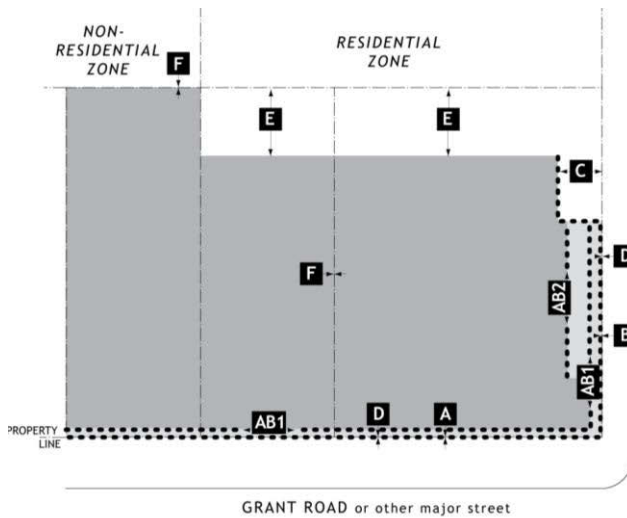
3. Grant Road District Categories



Figure 3.a: Grant Road District Zoning Category Map.



Center Marketplace



Street Frontage & Building Placement

Build-to Line ¹		
Along Major Streets	0'	A
Along other streets	0'	B
EXCEPT for:		
Private Storefront Frontage Type	up to 2'	AB1
Porch Frontage Type	up to 10'	AB2
Along all streets within 50' of a Residential Zone	prevailing <i>setback</i> of Residential Zone	C

Notes:

- For *individual lot frontages* on Major Streets, at least 65% of the lot frontage must have *building facade* at the *build-to line*.
- For *individual lot frontages* on existing non-Major Streets, at least 50% of the lot frontage must have *building facade* at the *build-to line*.
- Individual lot frontages* must have *building facade* at the *build-to line* within 50' of intersections involving a Major Street and within 30' of intersections involving a new street.

Perimeter Yard Minimums ^{1, 2}		
Street frontages	0'	D
Street frontages within 50' of a Residential Zone	Prevailing <i>setback</i> of Residential Zone	C
On Lot Interior:		
abutting Residential Zone	25'	E
abutting non-Residential Zone	0'	F
lots abutting alley may count one-half alley width toward <i>perimeter yard</i> requirement		G

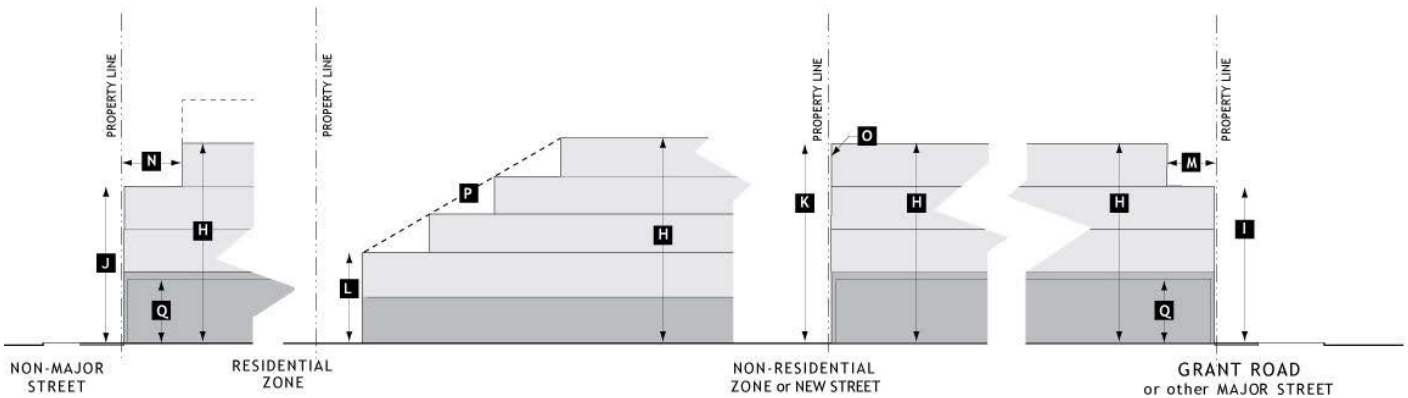
¹ See discussion of Minor Improvements for exceptions, on next page.
² See Frontage Types in Section 5.5.7 for *Perimeter Yard* Maximums.

Frontage	
Public Storefront	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Private Storefront	Allowed only on non-Major Streets except within 30' of their intersections with Major Streets
Arcade	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Porch	Allowed only on non-Major Streets except within 30' of their intersections with Major Streets
Public Yard	Allowed except within 30' of intersections of Grant Road with Major Streets
Private Yard	Allowed only on existing non-Major Streets except within 30' of their intersections with Major Streets
Courtyard	Allowed
Parking Lot	Allowed except within 30' of intersections involving Major Streets
Park or Plaza	Allowed

Notes:

- See Frontage Types Section 4.5.5 for standards for each Frontage Type.
- Vehicular drives prohibited along Major Streets within 30' of intersections.
- Frontage Types must also achieve *build-to line* requirements.
- Publicly accessible open spaces should be treated as non-Major Streets for the purposes of allowed Frontage Types.
- At least 80% of the building frontage along streets and open spaces shall contain active ground floor uses. Active uses include retail space, lobbies, entries, offices, living rooms, and kitchens of residential uses, common rooms and recreation spaces of institutional and public gathering uses, and other similarly active uses. These do not include storage areas, bathrooms, bedrooms, or other similarly less active or private uses.

Center Marketplace



Building Height, Massing & Articulation

Maximum Building Height		
Site Maximum	50'	H
On Grant Road and other Major Streets	10' less than site maximum	I
On non-Major Streets (general)	40'	J
On non-Major Streets (across from Residential Zones except within 50' of an intersection with a Major Street)	Match max of residential zone	
On new streets	50'	K
Abutting Residential Zones	25'	L
Abutting non-Residential Zones	50'	K
<ul style="list-style-type: none"> In Stone and Country Club Centers site maximum is 40' with additional 12' allowed by special Zoning Review Full Notice Procedure, see Section 2.6.3 of GRD. (H, K, L) 		

Minimum Upper Floor Stepbacks ³		
For heights above maximum allowed on Grant Road and other Major Streets to Site Maximum	10'	M
For heights above maximum allowed on non-Major Streets to Site Maximum	15'	N
For heights above maximum allowed on new streets to Site Maximum	0'	O
For heights above maximum allowed abutting Residential Zones:		P
Above 25' and up to 52' of building height	2' for every additional 1' of height ⁴	
For heights above maximum allowed abutting non-Residential Zones to Site Maximum	0'	O
Ground Floor Height		
Minimum clear ground floor ceiling height on <i>public frontages</i> for non-residential use	12'	Q

Minor Improvements:

For the purposes of this category, minor improvements are those where less than 50% of the existing buildings are demolished in the reuse of the property.

- Only new buildings and the portion of the property that is being redeveloped need comply with the Grant Road District *build-to line*, *perimeter yard*, frontage type, height, setback, and land use requirements.
- Compliance with multi-modal circulation requirements shall be through on-site circulation improvements (including sidewalks and landscaping) that begin to establish a pattern of blocks for the future revitalization of the property.

Multi-Modal Circulation:

- The character of the Center Marketplace category depends largely on the addition of new streets on the sites zoned with this category. New streets can create the framework of the transition of sites from auto-focused shopping centers to walkable mixed-use shopping areas.
- Blocks with Major Street faces longer than 300' must be bisected by new streets.
- See General Standards and Block and Circulation Standards for the design of new streets and other standards.

³ One-half of alley width may count toward setback distance.

⁴ Base point for setback measurement is minimum *perimeter yard*.



Center Marketplace

Permitted Land Uses

Agricultural Use Group	
Small Commercial Garden	
Community Garden	
Civic Use Group	
Civic Assembly	
Cultural Use	
Educational Use	
Membership Organization	
Postal Service	
Protective Service	
Religious Use	
Commercial Services Use Group	
Administrative and Professional Office	
Alcoholic Beverage Service	(6) (18) (24)
Animal Service	(6) (12) (13) (14) (23)
Automotive-Service and Repair: Minor	(6)
Communication	(6)
Day Care	
Entertainment	(6) (21) (24)
Financial Service	(6) (10)
Food Service	(6) (11) (18) (23) (28) (29)
Funeral Service	(6) (23) (32)
Medical Service-Extended Health Care	(6)
Medical Service Major	(6) (23)
Medical Service Outpatient	(6)
Parking	(6)
Personal Service	(6)
Technical Service	(6)
Trade Service and Repair, Minor	(6)
Travelers Accommodation, Lodging	(6)
Artisan Residence	

Industrial Use Group	
Craftwork	(6)
Perishable Goods Manufacturing	(6) (9) (23) (33)
Recreation Use Group	
Neighborhood Recreation	
Recreation	
Residential Use Group	
Family Dwelling	
Group Dwelling	(5) (25)
Residential Care Services	
Retail Trade Use Group	
Food and Beverage Sales	(6) (18)
General Merchandise Sales	Single use building footprint 40,000 s.f. max. (6) (17) (34)
Swap Meets and Auctions	Outdoor use area 15,000 s.f. max. (6) (24) (36)
Storage Use Group	
Hazardous Material Storage	(8) (24)
Personal Storage	(8) (19) (23) (38)
Utilities Use Group	
Distribution System	
Renewable Energy Generation	

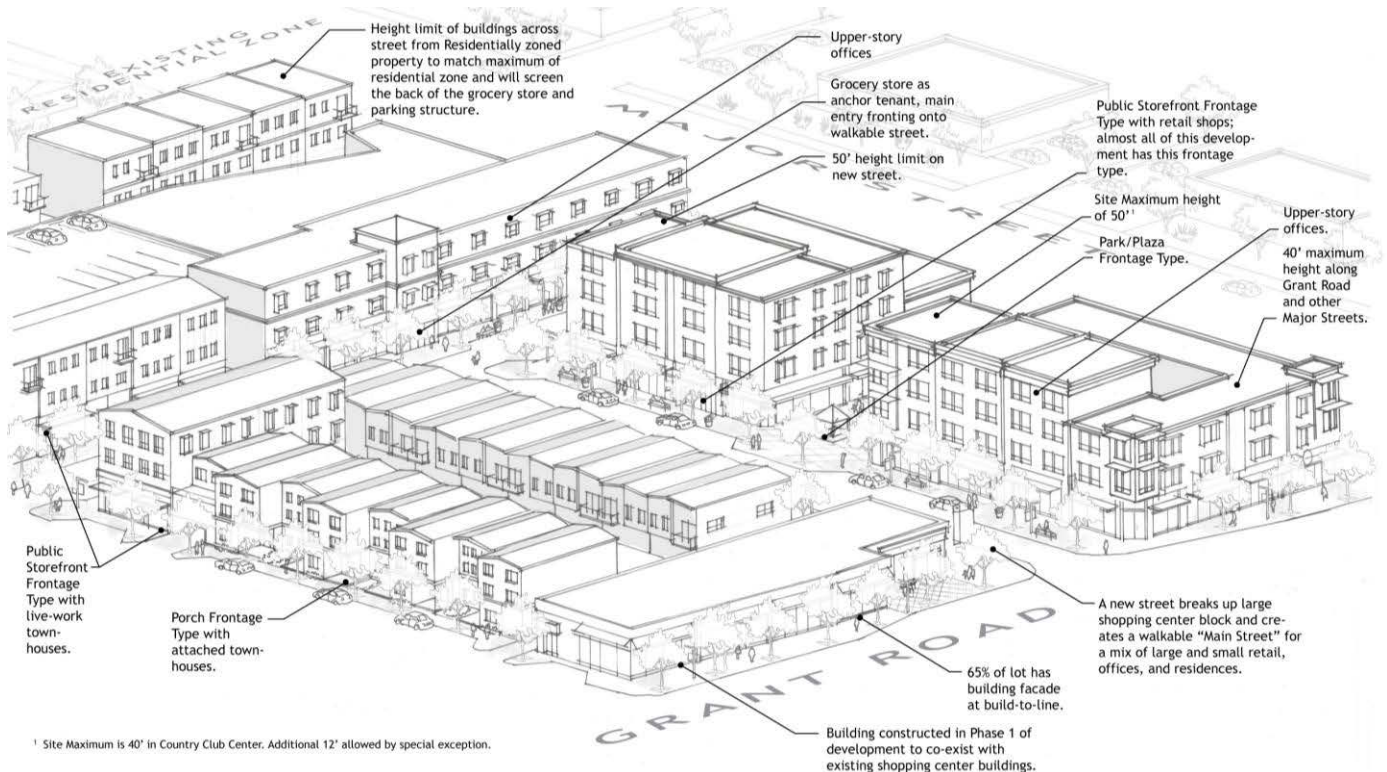
Notes:

- 5 – Not allowed when adjacent to or across the street from existing R-1 zoned area or existing single-family home.
- 6 – Use not allowed to front onto a non-major street with residential zoning across street unless within 100 feet of an intersection with a major street.
- 8 – Allowed only when associated with a business or residence on the same property.
- 9 – Allowed only when associated restaurant and/or retail facility.
- 10 – Drive through aisles or windows not allowed.
- 11 – Drive-in or drive-through restaurants or windows are not allowed.
- 12 – No boarding of animals permitted.
- 13 – Pet grooming and veterinary clinics permitted.
- 14 – All activities must be within a completely enclosed building.
- 17 – No vehicular fuel sales (i.e.; gas stations) permitted in the Fontana Center, Campbell Center, Country Club Center, and Columbus Center.

Center Marketplace

Illustrative View

One possible scenario showing how new and modified existing development could meet the development standards of the Center Marketplace category



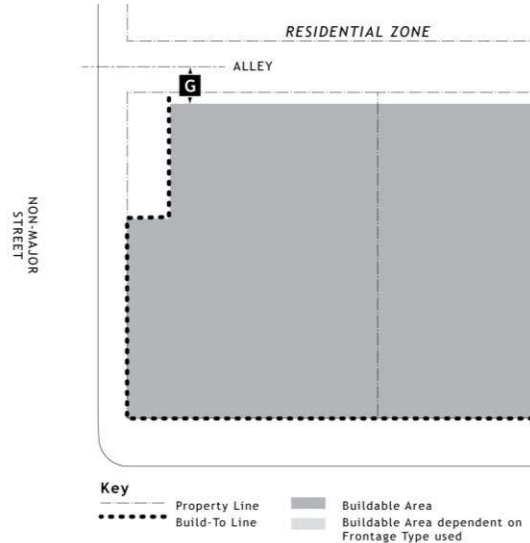
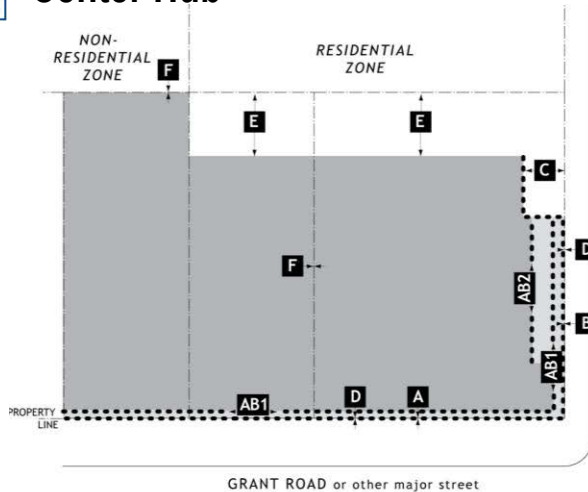
¹ Site Maximum is 40' in Country Club Center. Additional 12' allowed by special exception.

Notes (Continued):

- 18 – Conditional use when serving alcoholic beverages requiring a C-1 alcohol mitigation plan when within 50 feet of a residential zone or a residential use.
- 19 – Maximum storage area shall not be larger than 100 sq. ft. per dwelling unit and per 1,000 sq. ft. of business space.
- 20 – The operation of the artisan residence shall not create noise, vibrations, smoke, fumes, dust, odors, vapors, other noxious emissions, or any other similar nuisances that are discernible beyond the boundaries of the structure enclosing the artisan residence.
- 21 – Use shall not result in a noise measurement at an adjacent residentially zoned property equal to or higher than 55 dB(A) from 7:00 a.m. to 10:00 p.m. or 50 dB(A) from 10:00 p.m. to 7:00 a.m.
- 23 – Grant Road Modified Limited Notice Procedure.
- 24 – PSD Director approval through Limited Notice Procedure.
- 25 – Board of Adjustments approval through Full Notice Procedure when adjacent to existing R-1 zoned property, see Section 2.6.3 of GRD.
- 28 – Section 3.5.4.6 of the LUC applies.
- 29 – Section 3.5.4.7.A of the LUC applies.
- 32 – Section 3.5.4.22 of the LUC applies.
- 33 – Sections 3.5.5.2.A and 3.5.5.2.B of the LUC apply.
- 34 – Section 3.5.9.2.A of the LUC applies.
- 36 – Section 3.5.9.4 of the LUC applies.
- 38 – Section 3.5.10.3 of the LUC applies, except for 3.5.10.3.G.
- 39 – Sections 3.5.11.1. A, B, E, I, and K of the LUC apply.
- 40 – Section 3.5.11.2 of the LUC applies.



Center Hub



Street Frontage & Building Placement

Build-to Line		
Along Major Streets	0'	A
Along other streets	0'	B
EXCEPT for:		
Private Storefront Frontage Type	up to 2'	AB1
Porch Frontage Type	up to 10'	AB2
Along all streets within 50' of a Residential Zone	prevailing setback of Residential Zone	C

Frontage	
Public Storefront	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Private Storefront	Allowed only on non-Major Streets except within 30' from their intersections with Major Streets
Arcade	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Porch	Allowed only on non-Major Streets except within 30' from their intersections with Major Streets
Public Yard	Allowed except within 30' of intersections of Grant Road and other Major Streets. Must be active: used for display, dining, or other active uses
Private Yard	Allowed only on non-Major Streets except within 30' from their intersections with Major Streets
Courtyard	Allowed
Parking Lot	Allowed except within 30' of intersections involving a Major Street
Park or Plaza	Allowed

Notes:

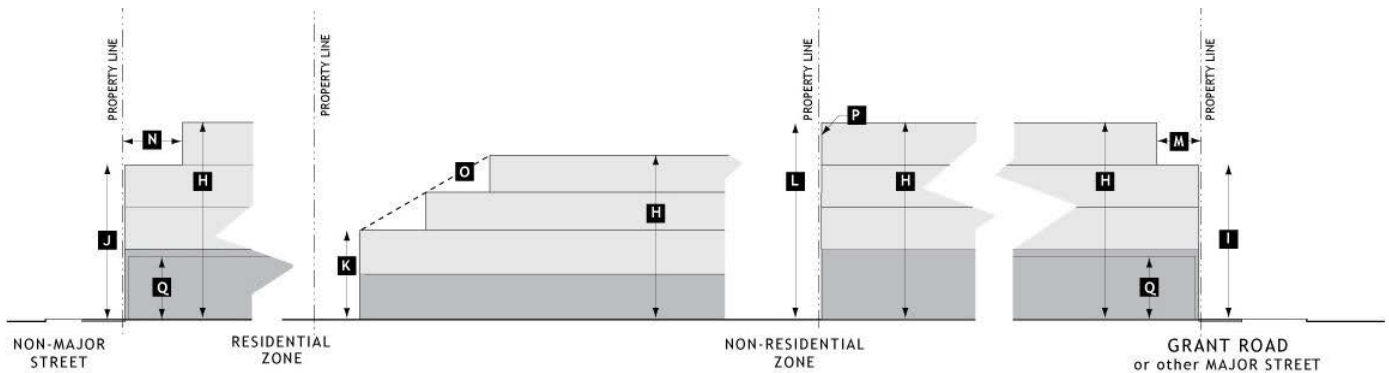
- For individual lot frontages on Major Streets, at least 80% of the lot frontage must have building facade at the build-to line.
- For individual lot frontages on existing non-Major Streets, at least 65% of the lot frontage must have building facade at the build-to line.
- Individual lot frontages must have building facade at the build-to line within 50' of street intersections involving a Major Street.

Perimeter Yard Minimums ¹		
Street Frontages	0'	D
Street frontages within 50' of a Residential Zone	Prevailing setback of Residential Zone	C
On Lot Interior:		
abutting Residential Zone	25'	E
abutting non-Residential Zone	0'	F
lots abutting alley may count one-half alley width toward perimeter yard requirement		G

Notes:

- See Frontage Types Section 4.5.5 for standards for each Frontage Type.
- Vehicular drives prohibited along Major Streets within 30' of intersections.
- Frontage Types must also achieve build-to line requirements.
- Publicly accessible open spaces should be treated as non-Major Streets for the purposes of allowed Frontage Types.
- Active ground floor uses are required on 80% of building facade on all streets and open space frontages. Active uses include retail space, lobbies, entries, offices, living rooms, and kitchens of residential uses, common rooms and recreation spaces of institutional and public gathering uses, and other similarly active uses. These do not include storage areas, bathrooms, bedrooms, or other similarly less active or private uses.

¹ See Frontage Types in Section 5.5.7 for Perimeter Yard Maximums.



Building Height, Massing & Articulation

Maximum Building Height		
Site Maximum	50'	H
On Grant Road and other Major Streets	10' less than site maximum	I
On non-Major Streets (general)	10' less than site max.	J
On non-Major Streets (across from Residential Zones)	Match max of residential zone	
Abutting Residential Zones	25'	K
Abutting non-Residential Zones	50'	L

- In Fontana, Tucson, and Oracle Centers site maximum is 40' with an additional 12' allowed by special Zoning Review Full Notice Procedure, see Section 2.6.3 of GRD. (H, L)
- Maximum Building Height (I) is 15' on Oracle Road

Minimum Upper Floor Stepbacks ²		
for heights above maximum allowed on Grant Road and other Major Streets to Site Maximum	10'	M
for heights above maximum allowed on non-Major Streets to Site Maximum	15'	N
for heights above maximum allowed abutting Residential Zones:		O
Above 25' and up to 52' of building height	2' for every additional 1' of height ³	
for heights above maximum allowed abutting non-Residential Zones to Site Maximum	0'	P

Ground Floor Height		
Minimum clear ground floor ceiling height on public frontages for non-residential use	12'	Q

² One-half of alley width may count toward setback distance.
³ Base point for setback measurement is minimum perimeter yard.



Center Hub

Permitted Land Uses

Agricultural Use Group

Small Commercial Garden

Community Garden

Civic Use Group

Civic Assembly

Cultural Use

Educational Use

Membership Organization

Postal Service

Protective Service

Religious Use

Commercial Services Use Group

Administrative and Professional Office

Alcoholic Beverage Service (6) (18) (24)

Animal Service (6) (12) (13) (14) (23)

Communications (6) (23) (27)

Construction Service (3) (6) (23)

Day Care

Entertainment (6) (21) (24)

Financial Services (6) (10)

Food Services (6) (11) (18) (23) (28) (29)

Medical Service-Extended Health Care (6)

Medical Service Outpatient (6)

Parking (6)

Personal Service (6)

Research and Product Development (1) (3) (6) (23) (30)

Technical Service (6)

Trade Service and Repair, Minor (6)

Travelers Accommodation, Lodging (6)

Artisan Residence (20) (23)

Industrial Use Group

Craftwork (6)

Recreation Use Group

Neighborhood Recreation

Recreation

Residential Use Group

Family Dwelling

Group Dwelling (5) (25)

Residential Care Services

Retail Trade Use Group

Food and Beverage Sales (6) (18)

General Merchandise Sales Single use building footprint 30,000 s.f. max. (6) (17) (23) (34)

Swap Meets and Auctions Outdoor use area 7,500 s.f. max. (6) (24) (36)

Storage Use Group

Hazardous Material Storage (7) (24)

Personal Storage (7) (19) (23) (38)

Utilities Use Group

Distribution System (23) (39)

Renewable Energy Generation (6) (24) (40)

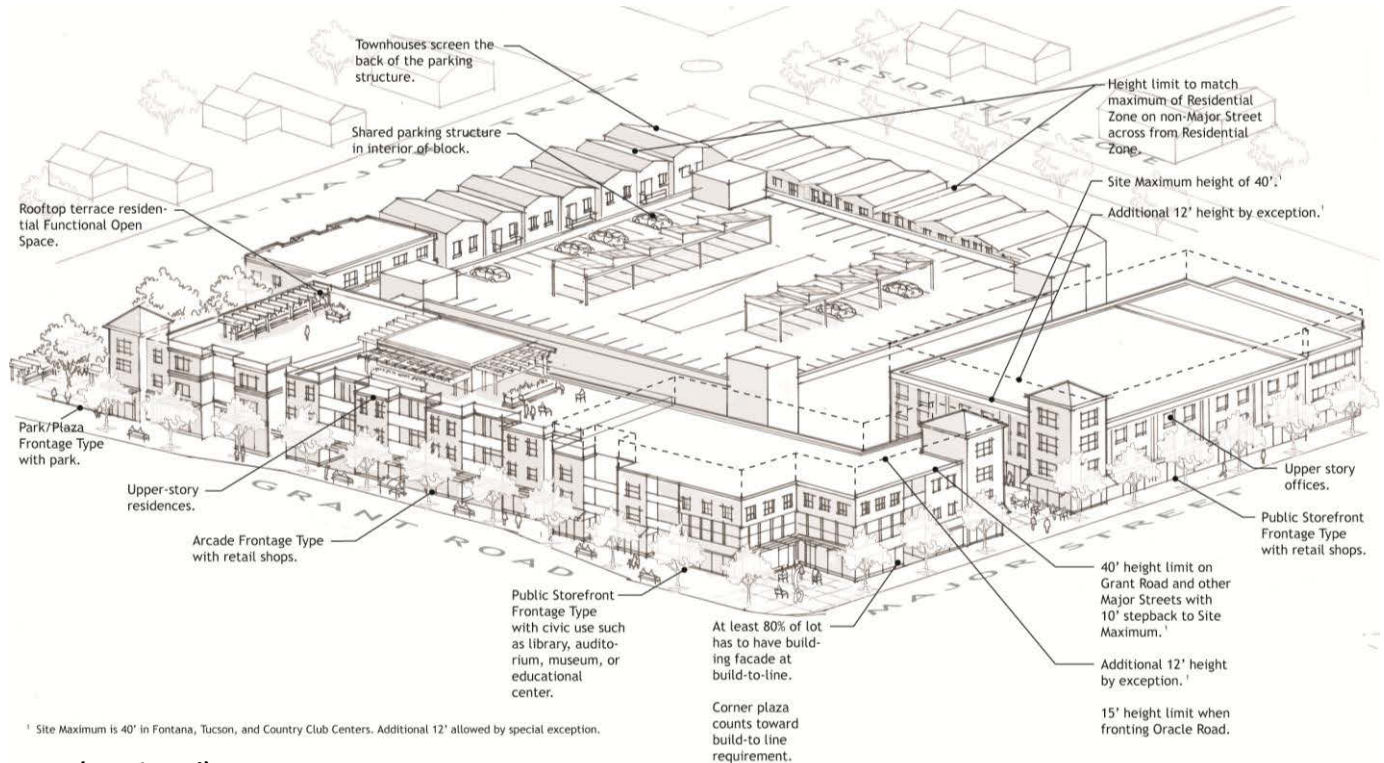
Notes:

- 1 – Not allowed in Country Club Center.
- 3 – Not allowed in Fontana Center.
- 5 – Not allowed when adjacent to or across the street from existing R-1 zoned area or existing single-family home.
- 6 – Use not allowed to front onto a non-major street with residential zoning across street unless within 100 feet of an intersection with a major street.
- 7 – Not allowed except as a use associated with a business or residence on the same property.
- 10 – Drive through aisles or windows not allowed.
- 11 – Drive-in or drive-through restaurants or windows are not allowed.
- 12 – No boarding of animals permitted.
- 13 – Pet grooming and veterinary clinics permitted.
- 14 – All activities must be within a completely enclosed building.

Center Hub

Illustrative View

One possible scenario showing how new and modified existing development could meet the development standards of the Center Hub category in a location other than the Fontana, Tucson, and Oracle Centers.

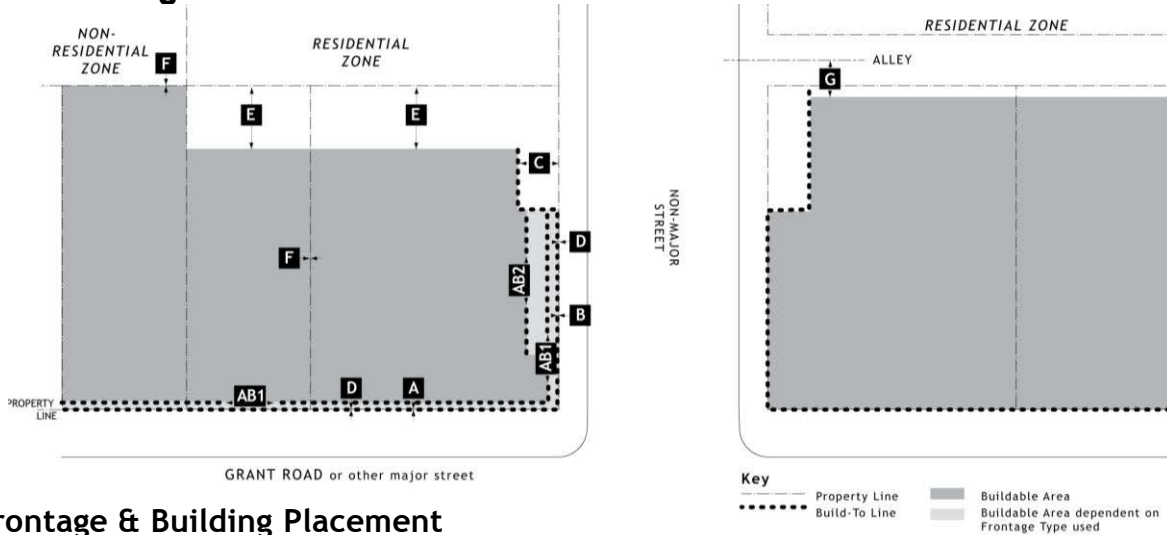


Notes (Continued):

- 17 – No vehicular fuel sales (i.e.; gas stations) permitted in the Fontana Center, Campbell Center, Country Club Center, and Columbus Center.
- 18 – Conditional use when serving alcoholic beverages requiring a C-1 alcohol mitigation plan when within 50 feet of a residential zone or a residential use.
- 19 – Maximum storage area shall not be larger than 100 sq. ft. per dwelling unit and per 1,000 sq. ft. of business space.
- 20 – The operation of the artisan residence shall not create noise, vibrations, smoke, fumes, dust, odors vapors, other noxious emissions, or any other similar nuisances that are discernible beyond the boundaries of the structure enclosing the artisan residence.
- 21 – Use shall not result in a noise measurement at an adjacent residentially zoned property equal to or higher than 55 dB(A) from 7:00 a.m. to 10:00 p.m. or 50 dB(A) from 10:00 p.m. to 7:00 a.m.
- 23 – Grant Road Modified Limited Notice Procedure.
- 24 – PDSO Director approval through Limited Notice Procedure.
- 25 – Board of Adjustments approval through Full Notice Procedure when adjacent to existing R-1 zoned property, see Section 2.6.3 of GRD.
- 27 – Allowed per Section 2.5.2.3 of the Tucson Land Use Code.
- 28 – Section 3.5.4.6 of the LUC applies.
- 29 – Section 3.5.4.7.A of the LUC applies.
- 30 – Section 3.5.4.14.A of the LUC applies.
- 34 – Section 3.5.9.2.A of the LUC applies.
- 36 – Section 3.5.9.4 of the LUC applies.
- 38 – Section 3.5.10.3 of the LUC applies, except for 3.5.10.3.G.
- 39 – Sections 3.5.11.1. A, B, E, I, and K of the LUC apply.
- 40 – Section 3.5.11.2 of the LUC applies.



Center Neighborhood



Street Frontage & Building Placement

Build-to Line		
Along Major Streets	0'	A
Along other streets	0'	B
EXCEPT for:		
Private Storefront Frontage Type	up to 5'	AB1
Porch Frontage Type	up to 10'	AB2
Along all streets within 50' of a Residential Zone	prevailing <i>setback</i> of Residential Zone	C

Frontage	
Public Storefront	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Arcade	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Porch	Allowed only on non-Major Streets except within 30' from their intersections with Major Streets
Public Yard	Allowed only for outdoor display and/or active uses such as dining
Private Yard	Allowed only on non-Major Streets except within 30' of their intersections with Major Streets
Courtyard	Allowed
Parking Lot	Allowed, except within 30' of intersections involving Major Streets
Park or Plaza	Allowed

Notes:

- For *individual lot frontages* on Major Streets, at least 60% of the lot frontage must have *building facade* at the *build-to line*.
- For *individual lot frontages* on non-Major Streets, at least 40% of the lot frontage must have *building facade* at the *build-to line*.
- Individual lot frontages* must be built to the *build-to line* within 50' of street intersections involving a Major Street.

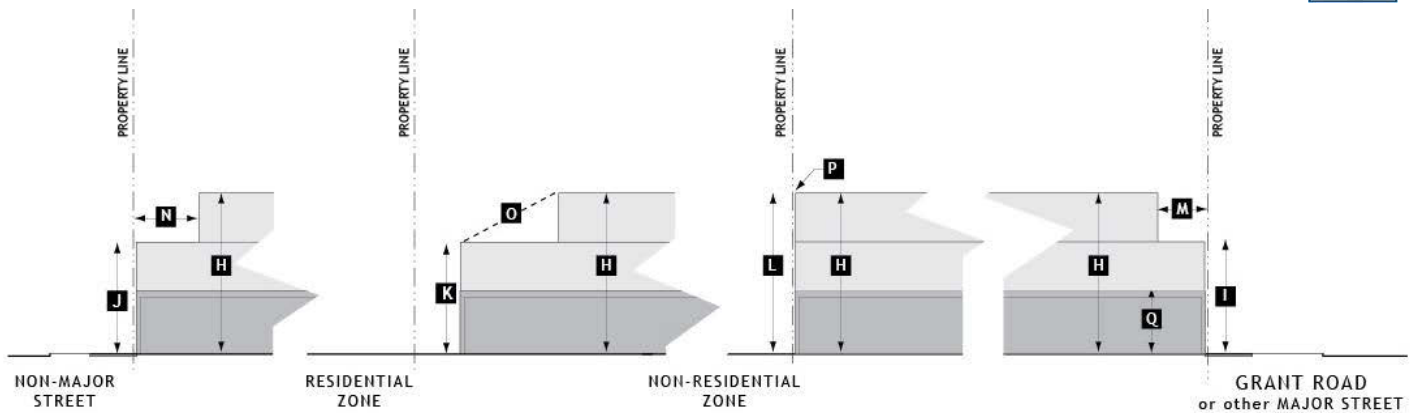
Notes:

- See Frontage Types in Section 4.5.5 for standards for each Frontage Type.
- Vehicular drives prohibited along Major Streets within 30' of intersections.
- Frontage Types must also achieve *build-to line* requirements.
- Publicly accessible open spaces should be treated as non-Major Streets for the purposes of allowed Frontage Types.
- Active ground floor uses are required on 80% of *building facade* on all streets and open space frontages. Active uses include retail space, lobbies, entries, offices, living rooms, and kitchens of residential uses, common rooms and recreation spaces of institutional and public gathering uses, and other similarly active uses. These do not include storage areas, bathrooms, bedrooms, or other similarly less active or private uses.

Perimeter Yard Minimums ¹		
Along Street Frontages	0'	D
Street frontages within 50' of a Residential Zone	Prevailing <i>setback</i> of Residential Zone	C
On Lot Interior:		
abutting Residential Zone	25'	E
abutting non-Residential Zone	0'	F
lots abutting alley can count one-half alley width toward <i>perimeter yard</i> requirement		G

¹ See Frontage Types in Section 5.5.7 for Perimeter Yard Maximums.

Center Neighborhood



Building Height, Massing & Articulation

Maximum Building Height

Site Maximum	40'	H
On Grant Road and other Major Streets	28'	I
On non-Major Streets	25' Across from residential, match max. of residential zone	J

Abutting Residential Zones	25' except within and adjacent to Jefferson Park NR-1, where height shall match the requirements of the neighborhood design standards	K
Abutting non-Residential Zones	40'	L

Minimum Upper Floor Stepbacks ²

for heights above maximum allowed on Grant Road and other Major Streets to Site Maximum	12'	M
for heights above maximum allowed on non-Major Streets to Site Maximum	12'	N
for heights above maximum allowed abutting Residential Zones:		O
Above 25' to Site Maximum building height	2' for every additional 1' of height ³	
for heights above maximum allowed abutting non-Residential Zones to Site Maximum	0'	P

Ground Floor Height

Minimum clear ground floor ceiling height on public frontages for non-residential use	12'	Q
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² One-half of alley width may count toward stepback distance.
³ Base point for stepback measurement is minimum perimeter yard.



Center Neighborhood

Permitted Land Uses

Agricultural Use Group

Small Commercial Garden

Community Garden

Civic Use Group

Civic Assembly

Cultural Use

Educational Use

Membership Organization

Postal Service

Protective Service

Religious Use

Commercial Services Use Group

Administrative and Professional Office

Alcoholic Beverage Service Single use building area 5,000 s.f. max. (6)(18)(26)

Animal Service (6) (13) (14) (15) (23)

Automotive - Service and Repair: Minor (6) (16) (23)

Communications (6) (23) (27)

Day Care Single use building area 7,500 s.f. max.

Entertainment (6) (22) (24)

Financial Services (6) (10)

Food Services Single use building area 10,000 s.f. max. (6) (11) (18) (23) (28) (29)

Medical Service-Extended Health Care (6)

Medical Service Outpatient (6)

Parking 50 spaces max. per lot (6) (23)

Personal Service Single use building area 5,000 s.f. max. (6) (23)

Technical Service Single use building area 5,000 s.f. max. (6) (23)

Trade Service and Repair, Minor Single use building area 5,000 s.f. max. (6) (23)

Travelers Accommodation, Lodging (6)

Artisan Residence (20) (23)

Industrial Use Group

Craftwork (6)

Recreation Use Group

Neighborhood Recreation

Recreation Single use building area 20,000 s.f. max. (23)

Residential Use Group

Family Dwelling

Group Dwelling (5) (25)

Residential Care Services

Retail Trade Use Group

Food and Beverage Sales Single use building area 12,500 s.f. max. (6) (18) (24)

General Merchandise Sales Single use building area 15,000 s.f. max. (6) (17) (23) (34)

Swap Meets and Auctions Outdoor use area 7,500 s.f. max. (6) (24) (36)

Storage Use Group

Hazardous Material Storage (7) (24)

Personal Storage (7) (19) (23) (38)

Utilities Use Group

Renewable Energy Generation (6) (24) (40)

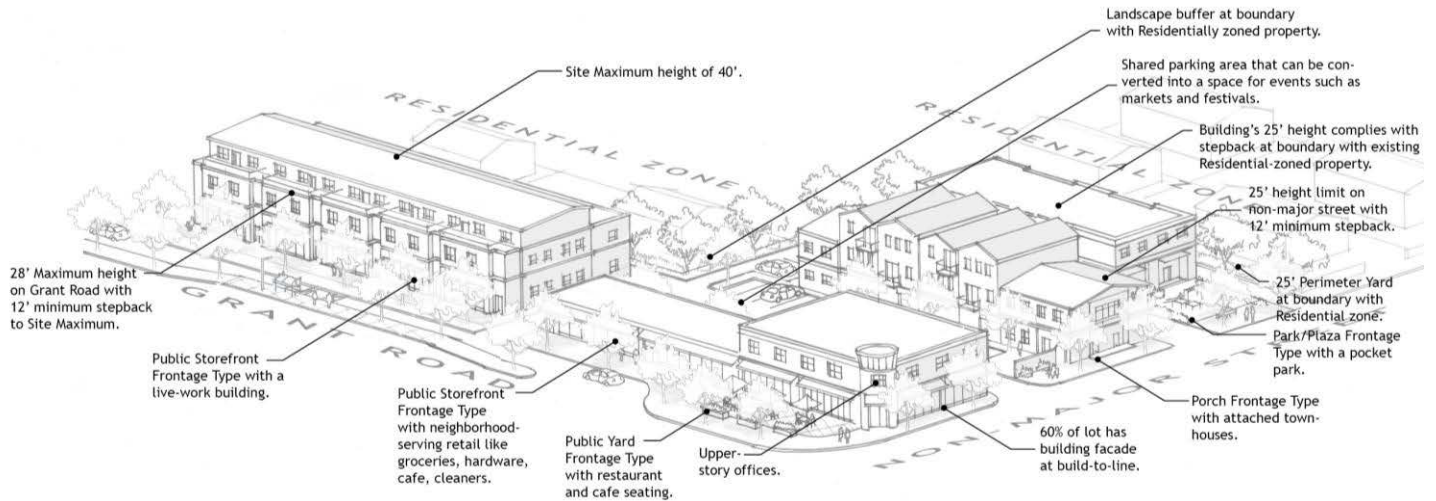
Notes:

- 5 – Not allowed when adjacent to or across the street from existing R-1 zoned area or existing single-family home.
- 6 – Use not allowed to front onto a non-major street with residential zoning across street unless within 100 feet of an intersection with a major street.
- 7 – Not allowed except as a use associated with a business or residence on the same property.
- 10 – Drive through aisles or windows not allowed.
- 11 – Drive-in or drive-through restaurants or windows are not allowed.

Center Neighborhood

Illustrative View

One possible scenario showing how new and modified existing development could meet the development standards of the Center Neighborhood category

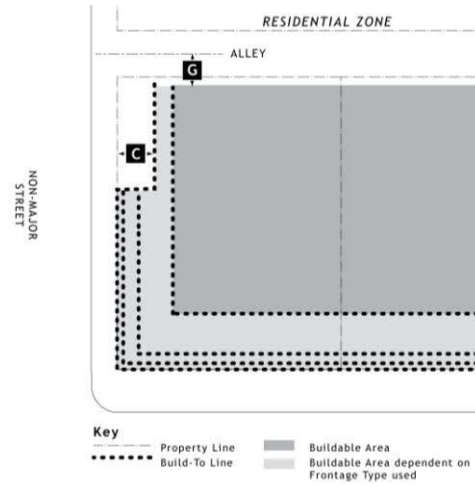
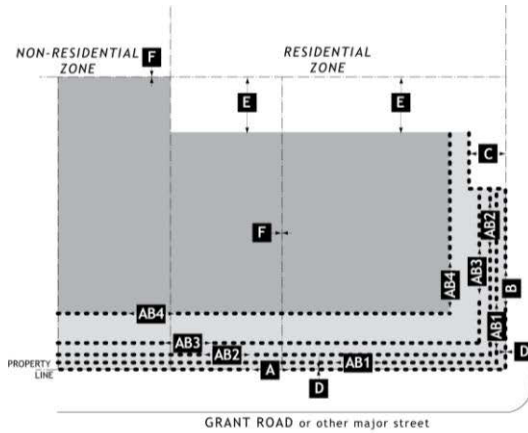


Notes (Continued):

- 13 – Pet grooming and veterinary clinics permitted.
- 14 – All activities must be within a completely enclosed building.
- 15 – Overnight confinement for clinic treatment is permitted for a maximum of five (5) animals.
- 16 – No car washing.
- 17 – No vehicular fuel sales (i.e.; gas stations) permitted in the Fontana Center, Campbell Center, Country Club Center, and Columbus Center.
- 18 – Conditional use when serving alcoholic beverages requiring a C-1 alcohol mitigation plan when within 50 feet of a residential zone or a residential use.
- 19 – Maximum storage area shall not be larger than 100 sq. ft. per dwelling unit and per 1,000 sq. ft. of business space..
- 20 – The operation of the artisan residence shall not create noise, vibrations, smoke, fumes, dust, odors vapors, other noxious emissions, or any other similar nuisances that are discernible beyond the boundaries of the structure enclosing the artisan residence.
- 22 – Allowed as an associated use within a restaurant or café fully within a building and use shall not result in a noise measurement at an adjacent residentially zone property equal to or higher than 50dB(A) from 7:00 a.m. to 10:00 p.m. or 40dB(A) from 10:00 p.m. to 7:00 a.m.
- 23 – Grant Road Modified Limited Notice Procedure.
- 24 – PDSB Director approval through Limited Notice Procedure.
- 25 – Board of Adjustments approval through Full Notice Procedure when adjacent to existing R-1 zoned property, see Section 2.6.3 of GRD.
- 26 – Zoning Examiner approval through Limited Notice Procedure.
- 27 – Allowed per Section 2.5.2.3 of the Tucson Land Use Code.
- 28 – Section 3.5.4.6 of the LUC applies.
- 29 – Section 3.5.4.7.A of the LUC applies.
- 34 – Section 3.5.9.2.A of the LUC applies.
- 36 – Section 3.5.9.4 of the LUC applies.
- 38 – Section 3.5.10.3 of the LUC applies, except for 3.5.10.3.G.
- 39 – Sections 3.5.11.1. A, B, E, I, and K of the LUC apply.
- 40 – Section 3.5.11.2 of the LUC applies.



Center General



Street Frontage & Building Placement

Build-to Line		
Along Major Streets	0'	A
Along other streets	0'	B
EXCEPT for:		
Public Storefront Frontage Type	up to 2'	AB1
Private Storefront Frontage Type	up to 5'	AB2
Porch Frontage Type	up to 15'	AB3
Public Yard Frontage Type	up to 25'	AB4
Along all streets within 50' of a Residential Zone	prevailing <i>setback</i> of Residential Zone	C

Notes:

- For individual lot frontages on Major Streets, at least 50% of the lot frontage must have *building facade* at the *build-to line*.
- For individual lot frontages on non-Major Streets, at least 35% of the lot frontage must have *building facade* at the *build-to line*.
- Individual lot frontages must have *building facade* at the *build-to line* within 30' of street intersections involving a Major Street.
- The Shared Motorcourt Frontage Type is an exception to the **Build-To-Line** standards; For Shared Motorcourts only 40% of the lot frontage must have *building facade* at the *build-to-line*.

Perimeter Yard Minimums ¹		
Along Street Frontages	0'	D
Street frontages within 50' of a Residential Zone	Prevailing <i>setback</i> of Residential Zone	C
On Lot Interior:		
abutting Residential Zone	25'	E
abutting non-Residential Zone	0'	F
lots abutting alley can count alley one-half width toward perimeter yard requirement		G

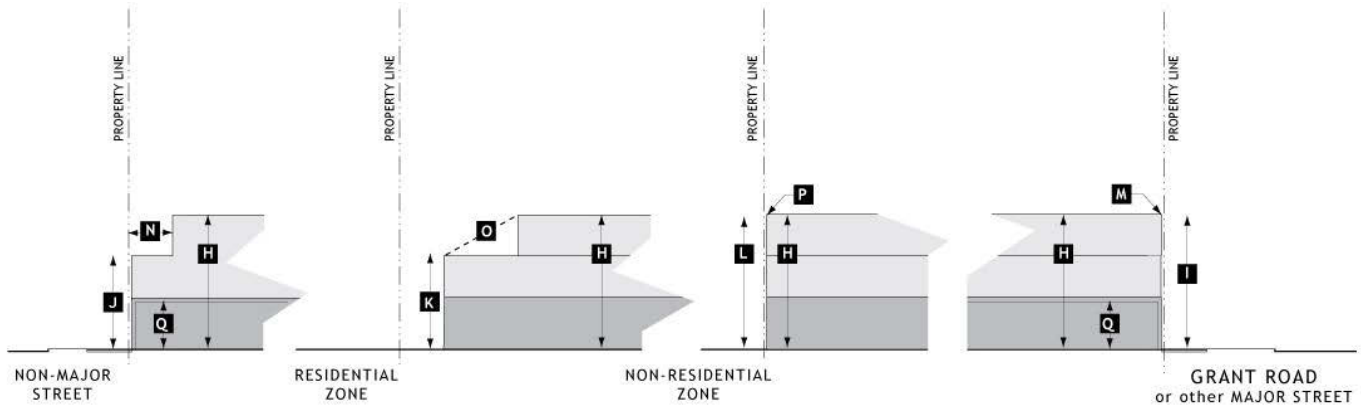
¹ See Frontage Types in Section 5.5.7 for Perimeter Yard Maximums.

Frontage	
Public Storefront	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Private Storefront	Allowed, except within 30' of intersections of Grant Road and other Major Streets
Arcade	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Porch	Allowed, except within 30' from intersections involving a Major Street
Public Yard	Allowed
Private Yard	Allowed, except within 30' of an intersection involving a Major Street
Shared Motorcourt	Allowed
Courtyard	Allowed
Parking Lot	Allowed
Park or Plaza	Allowed

Notes:

- See Frontage Types Section 4.5.5 for standards for each Frontage Type.
- Vehicular drives prohibited along Major Streets within 30' of intersections.
- Use of Frontage Types must also achieve *build-to line* requirements, however some Frontage Types adjust the location of the required *build-to line*; see *Build-to Line* section.
- Publicly accessible open spaces should be treated as non-Major Streets for the purposes of allowed Frontage Types.
- Active ground floor uses are required on 80% of *building facade* on all streets and open space frontages. Active uses include retail space, lobbies, entries, offices, living rooms, and kitchens of residential uses, common rooms and recreation spaces of institutional and public gathering uses, and other similarly active uses. These do not include storage areas, bathrooms, bedrooms, or other similarly less active or private uses.

Center General



Building Height, Massing & Articulation

Maximum Building Height		
Site Maximum	40'	H
On Grant Road and other Major Streets	40'	I
On non-Major Streets	25' Across from residential, match max. of residential zone	J
Abutting Residential Zones	25'	K
Abutting non-Residential Zones	40'	L
<ul style="list-style-type: none"> In Oracle and Stone Centers site maximum is 45' (H, I, L) 		
Minimum Upper Floor Stepbacks ²		
for heights above maximum allowed on Grant Road and other Major Streets to Site Maximum	0'	M
for heights above maximum allowed on non-Major Streets to Site Maximum	12'	N
for heights above maximum allowed abutting Residential Zones: Above 25' to Site Maximum building height	2' for every additional 1' of height ³	O
for heights above maximum allowed abutting non-Residential Zones to Site Maximum	0'	P
Ground Floor Height		
Minimum clear ground floor ceiling height on public frontages for non-residential use	12'	Q

² One-half alley width may count toward stepback distance.

³ Base point for stepback measurement is minimum perimeter yard.



Center General

Permitted Land Uses

Agricultural Use Group	
Small Commercial Garden	(23)
Community Garden	
Civic Use Group	
Civic Assembly	
Cultural Use	
Educational Use	
Membership Organization	
Postal Service	
Protective Service	
Religious Use	
Commercial Services Use Group	
Administrative and Professional Office	
Alcoholic Beverage Service	Single use building area 5,000 s.f. max. (6)(18)(26)
Animal Service	(6) (12) (14) (23)
Automotive - Service and Repair: Minor	(1) (2) (6) (23)
Building and Grounds Maintenance	(1) (2) (6) (23)
Communications	(6)
Construction Services	(4) (6) (23)
Day Care	
Financial Services	(6) (10)
Food Services	(6) (11) (18) (23) (28)(29)
Funeral Service	(6) (23) (32)
Medical Service-Extended Health Care	(6)
Medical Service Major	(6)
Medical Service Outpatient	(6)
Parking	(6)
Personal Service	(6)
Technical Service	(6)
Trade Service and Repair, Minor	(6)
Travelers Accommodation, Lodging	(6)
Artisan Residence	(20) (23)

Industrial Use Group	
Craftwork	(6)
Perishable Goods Manufacturing	(6) (9) (23) (33)
Recreation Use Group	
Neighborhood Recreation	
Recreation	Single use building area 10,000 s.f. max. (23)
Residential Use Group	
Family Dwelling	
Group Dwelling	(5) (25)
Residential Care Services	
Retail Trade Use Group	
Construction Material Sales	(6)
Food and Beverage Sales	(6) (18)
General Merchandise Sales	Single use building footprint 30,000 s.f. max. (6) (17) (23) (34)
Swap Meets and Auctions	Outdoor use area 15,000 s.f. max. (6) (24) (36)
Storage Use Group	
Hazardous Material Storage	(7) (24)
Personal Storage	(7) (19) (23) (38)
Utilities Use Group	
Distribution System	(23) (39)
Renewable Energy Generation	(6) (24) (40)
Wholesale Use Group	
Business Supply and Equip. Wholesaling	(1) (2) (6) (23)

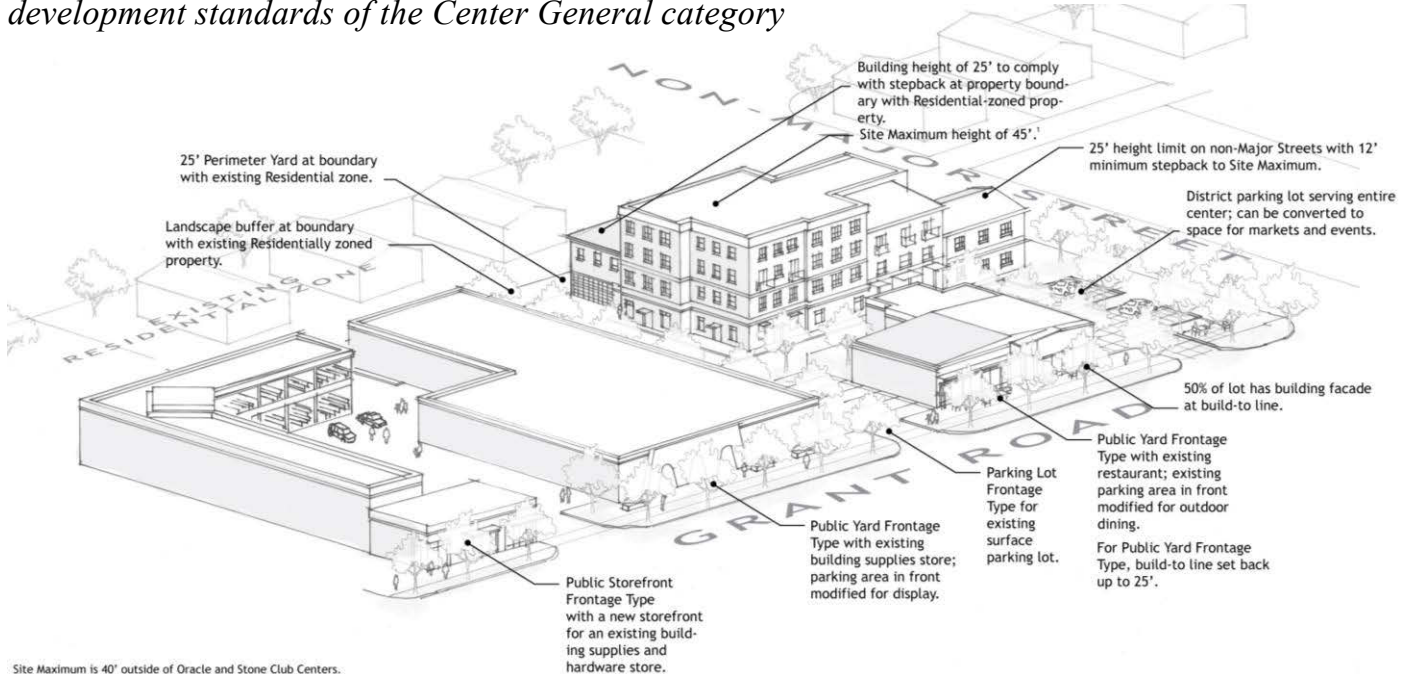
Notes:

- 1 – Not allowed in Country Club Center.
- 2 – Not allowed in Columbus Center.
- 4 – Allowed only in Oracle and Stone Centers.
- 5 – Not allowed when adjacent to or across the street from existing R-1 zoned area or existing single-family home.
- 6 – Use not allowed to front onto a non-major street with residential zoning across street unless within 100 feet of an intersection with a major street.
- 7 – Not allowed except as a use associated with a business or residence on the same property.
- 9 – Allowed only when associated restaurant and/or retail facility.

Center General

Illustrative View

One possible scenario showing how new and modified existing development could meet the development standards of the Center General category

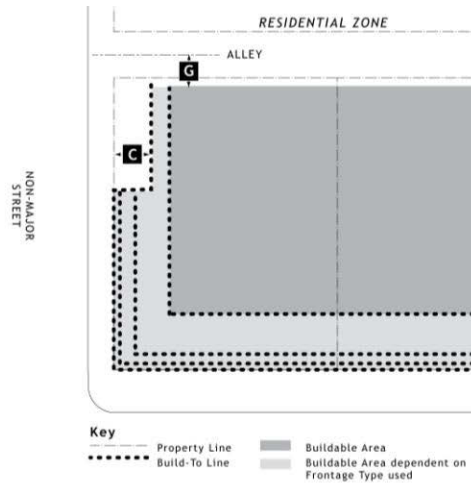
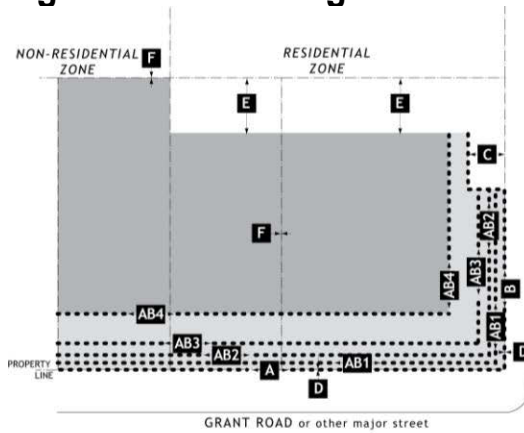


Notes (Continued):

- 10 – Drive through aisles or windows not allowed.
- 11 – Drive-in or drive-through restaurants or windows are not allowed.
- 12 – No boarding of animals permitted.
- 14 – All activities must be within a completely enclosed building.
- 17 – No vehicular fuel sales (i.e.; gas stations) permitted in the Fontana Center, Campbell Center, Country Club Center, and Columbus Center.
- 18 – Conditional use when serving alcoholic beverages requiring a C-1 alcohol mitigation plan when within 50 feet of a residential zone or a residential use.
- 19 – Maximum storage area shall not be larger than 100 sq. ft. per dwelling unit and per 1,000 sq. ft. of business space.
- 20 – The operation of the artisan residence shall not create noise, vibrations, smoke, fumes, dust, odors vapors, other noxious emissions, or any other similar nuisances that are discernible beyond the boundaries of the structure enclosing the artisan residence.
- 23 – Grant Road Modified Limited Notice Procedure.
- 24 – PDS Director approval through Limited Notice Procedure.
- 31 – Section 3.5.10.3 of the LUC applies, except for 3.5.10.3.G.
- 25 – Board of Adjustments approval through Full Notice Procedure when adjacent to existing R-1 zoned property, see Section 2.6.3 of GRD.
- 26 – Zoning Examiner approval through Limited Notice Procedure.
- 28 – Section 3.5.4.6 of the LUC applies.
- 29 – Section 3.5.4.7.A of the LUC applies.
- 32 – Section 3.5.4.22 of the LUC applies.
- 33 – Sections 3.5.5.2.A and 3.5.5.2.B of the LUC apply.
- 34 – Section 3.5.9.2.A of the LUC applies.
- 36 – Section 3.5.9.4 of the LUC applies.
- 38 – Section 3.5.10.3 of the LUC applies, except for 3.5.10.3.G.
- 39 – Sections 3.5.11.1. A, B, E, I, and K of the LUC apply.
- 40 – Section 3.5.11.2 of the LUC applies.



Segment West & Segment East



Street Frontage & Building Placement

Build-to Line		
Along Major Streets	0'	A
Along other streets	0'	B
EXCEPT for:		
Public Storefront Frontage Type	up to 2'	AB1
Private Storefront Frontage Type	up to 5'	AB2
Porch Frontage Type	up to 15'	AB3
Public Yard Frontage Type	up to 25'	AB4
Along all streets within 50' of a Residential Zone	prevailing <i>setback</i> of Residential Zone	C

Notes:

- For individual lot frontages on Major Streets, at least 50% of the lot frontage must have *building facade* at the *build-to line*.
- For individual lot frontages on non-Major Streets, at least 30% of the lot frontage must have *building facade* at the *build-to line*.
- Individual lot frontages must have *building facade* at the *build-to line* within 30' of street intersections involving a Major Street.
- The Shared Motorcourt Frontage Type is an exception to the above standards; For Shared Motorcourts only 40% of the lot frontage must have *building facade* at the *build-to line*.

Perimeter Yard Minimums ¹		
Along Street Frontages	0'	D
Street frontages within 50' of a Residential Zone	Prevailing <i>setback</i> of Residential Zone	C
On Lot Interior:		
abutting Residential Zone	25'	E
abutting non-Residential Zone	0'	F
lots abutting alley can count one-half alley width toward perimeter yard requirement		G

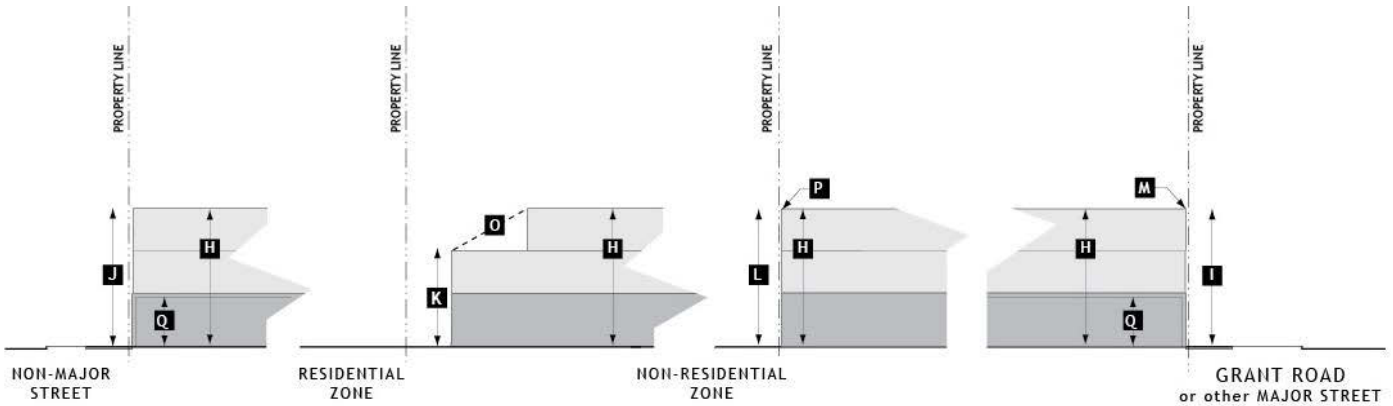
Frontage	
Public Storefront	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Private	Allowed
Arcade	Allowed, except when fronting a non-major street more than 100 feet from an intersection with a major street where there is an existing residentially-zoned area across the non-major street
Porch	Allowed
Public Yard	Allowed
Private Yard	Allowed
Courtyard	Allowed
Shared	Allowed
Service	Allowed only in Segment West
Parking Lot	Allowed
Park or Plaza	Allowed

Notes:

- See Frontage Types Section 4.5.5 for standards for each Frontage Type.
- Vehicular drives prohibited along Major Streets within 30' of intersections.
- Use of Frontage Types must also achieve *build-to line* requirements, however some Frontage Types adjust the location of the required *build-to line*; see *Build-to Line* section.
- Publicly accessible open spaces should be treated as non-Major Streets for the purposes of allowed Frontage Types.
- Active ground floor uses are required on 80% of *building facade* on all streets and open space frontages. Active uses include retail space, lobbies, entries, offices, living rooms, and kitchens of residential uses, common rooms and recreation spaces of institutional and public gathering uses, and other similarly active uses. These do not include storage areas, bathrooms, bedrooms, or other similarly less active or private uses.

¹ See Frontage Types in Section 5.5.7 for *Perimeter Yard* Maximums.

Segment West & Segment East



Building Height, Massing & Articulation

Maximum Building Height		
Site Maximum	40'	H
On Grant Road and other Major Streets	40'	I
On non-Major Streets in Segment West On non-Major Streets in Segment East	40' 25' Across from residential, match max. of residential zone	J
Abutting Residential Zones	25'	K
Abutting non-Residential Zones	40'	L
Minimum Upper Floor Stepbacks ²		
for heights above maximum allowed on Grant Road and other Major Streets to Site Maximum	0'	M
for heights above maximum allowed on non-Major Streets to Site Maximum	12'	N
for heights above maximum allowed abutting Residential Zones: Above 25' to Site Maximum building height	2' for every additional 1' of height ³	O
for heights above maximum allowed abutting non-Residential Zones to Site Maximum	0'	P
Ground Floor Height		
Minimum clear ground floor ceiling height on public frontages for non-residential use	12'	Q

² One-half alley width may count toward stepback distance.

³ Base point for stepback measurement is minimum perimeter yard.



Segment West & Segment East

Permitted Land Uses

Agricultural Use Group

Small Commercial Garden	(23)
Community Garden	

Civic Use Group

Civic Assembly	
Cultural Use	
Educational Use	
Membership Organization	
Postal Service	
Protective Service	
Religious Use	

Commercial Services Use Group

Administrative and Professional Office	
Alcoholic Beverage Service	(6)(18)(26)
Animal Service	(6) (14) (15) (23)
Automotive - Service and Repair: Minor	(6)
Automotive - Service and Repair: Major	(6) (24)
Building and Grounds Maintenance	(6)
Communications	(6)
Construction Services	(6)
Day Care	
Entertainment-Segment East Only	(6) (21) (24)
Financial Services-Segment East Only	(6) (10)
Food Services	(6) (11) (18) (23) (28) (29)
Funeral Service	(6) (23) (32)
Medical Service-Extended Health Care	(6)
Medical Service Major	(6)
Medical Service Outpatient	(6)
Parking	(6)
Personal Service	(6)
Research and Product Development	(6) (23) (30)
Technical Service	(6)
Trade Service and Repair Minor	(6)

Trade Service and Repair Major-Segment West Only	(6) (31)
Travelers Accommodation, Lodging	(6)
Artisan Residence	(20) (23)

Industrial Use Group

Craftwork	(6)
General Manufacturing	(6) (24)
Motion Picture Industry	(6) (21) (24)
Perishable Goods Manufacturing-Segment West Only	(6) (23) (33)
Precision Manufacturing	(6) (23)

Recreation Use Group

Neighborhood Recreation	
Recreation	

Residential Use Group

Family Dwelling	
Group Dwelling	(5) (25)
Residential Care Services	

Retail Trade Use Group

Construction Material Sales	(6)
Food and Beverage Sales	(6) (18)
General Merchandise Sales	Single use building footprint 25,000 s.f. max. (6) (23) (34)
Heavy Equipment Sales	(6) (23) (35)
Swap Meets and Auctions	Outdoor use area 15,000 s.f. max. (6) (24) (36)
Vehicle Rental and Sales	(6) (23) (37)

Storage Use Group

Hazardous Material Storage	(7) (24)
Personal Storage	(7) (19) (23) (38)

Utilities Use Group

Distribution System	(23) (39)
Renewable Energy Generation	(6) (24) (40)

Wholesale Use Group

Business Supply and Equip. Wholesaling	(6)
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Segment West & Segment East

Notes:

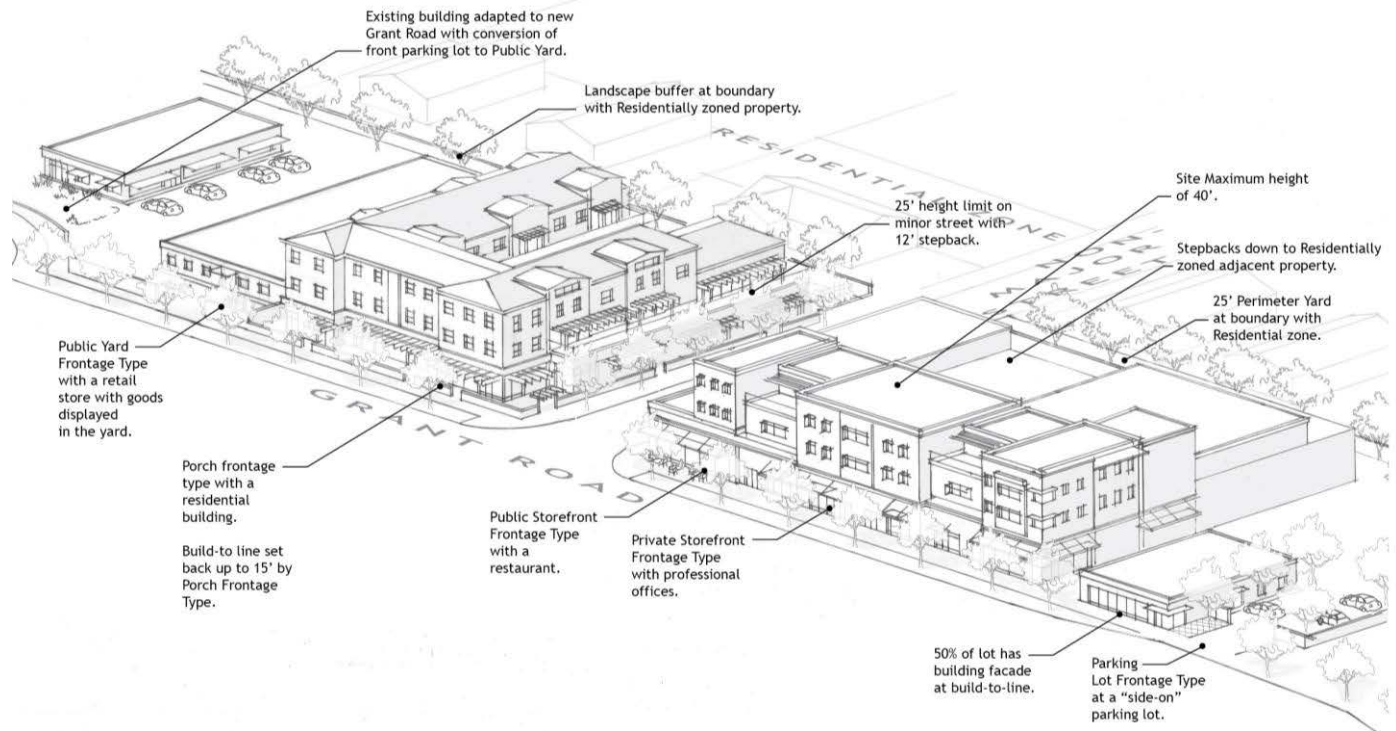
- 5 – Not allowed when adjacent to or across the street from existing R-1 zoned area or existing single-family home.
- 6 – Use not allowed to front onto a non-major street with residential zoning across street unless within 100 feet of an intersection with a major street.
- 7 – Not allowed except as a use associated with a business or residence on the same property.
- 10 – Drive through aisles or windows not allowed.
- 11 – Drive-in or drive-through restaurants or windows are not allowed.
- 14 – All activities must be within a completely enclosed building.
- 15 – Overnight confinement for clinic treatment is permitted for a maximum of five (5) animals.
- 18 – Conditional use when serving alcoholic beverages requiring a C-1 alcohol mitigation plan when within 50 feet of a residential zone or a residential use.
- 19 – Maximum storage area shall not be larger than 100 sq. ft. per dwelling unit and per 1,000 sq. ft. of business space.
- 20 – The operation of the artisan residence shall not create noise, vibrations, smoke, fumes, dust, odors vapors, other noxious emissions, or any other similar nuisances that are discernible beyond the boundaries of the structure enclosing the artisan residence.
- 21 – Use shall not result in a noise measurement at an adjacent residentially zoned property equal to or higher than 55 dB(A) from 7:00 a.m. to 10:00 p.m. or 50 dB(A) from 10:00 p.m. to 7:00 a.m.
- 23 – Grant Road Modified Limited Notice Procedure.
- 24 – PDS Director approval through Limited Notice Procedure.
- 25 – Board of Adjustments approval through Full Notice Procedure when adjacent to existing R-1 zoned property, see Section 2.6.3 of GRD.
- 26 – Zoning Examiner approval through Limited Notice Procedure.
- 28 – Section 3.5.4.6 of the LUC applies.
- 29 – Section 3.5.4.7.A of the LUC applies.
- 30 – Section 3.5.4.14.A of the LUC applies.
- 31 – Section 3.5.4.15 of the LUC applies.
- 32 – Section 3.5.4.22 of the LUC applies.
- 33 – Sections 3.5.5.2.A and 3.5.5.2.B of the LUC apply.
- 34 – Section 3.5.9.2.A of the LUC applies.
- 35 – Section 3.5.9.3 of the LUC applies.
- 36 – Section 3.5.9.4 of the LUC applies.
- 37 – Sections 3.5.9.5.A and 3.5.9.5.B of the LUC apply.
- 38 – Section 3.5.10.3 of the LUC applies, except for 3.5.10.3.G.
- 39 – Sections 3.5.11.1. A, B, E, I, and K of the LUC apply.
- 40 – Section 3.5.11.2 of the LUC applies.



Segment West & Segment East

Illustrative View

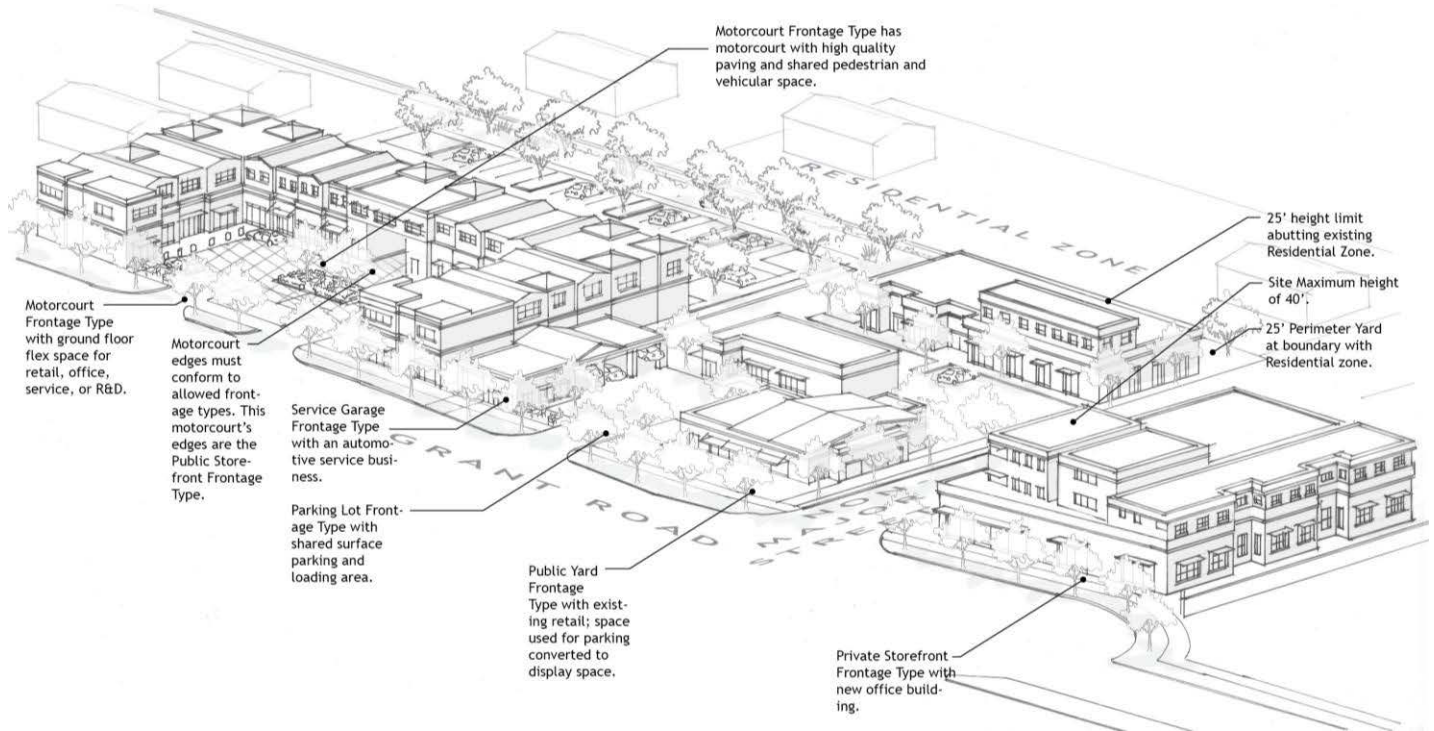
One possible scenario showing how new and modified existing development could meet the development standards of the Segment East category



Segment West & Segment East

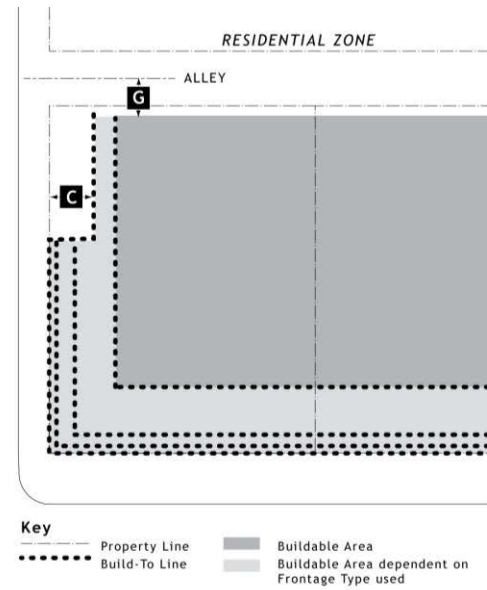
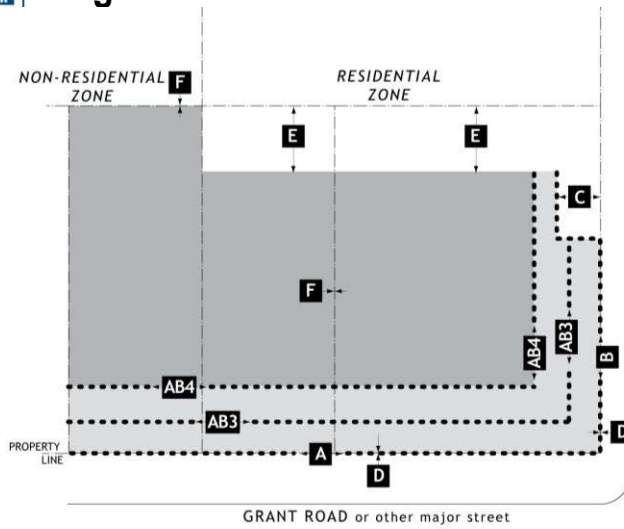
Illustrative View

One possible scenario showing how new and modified existing development could meet the development standards of the Segment West category





Segment Central



Street Frontage & Building Placement

Build-to Line		
Along Major Streets	0'	A
Along other streets	0'	B
EXCEPT for:		
Porch Frontage Type	up to 15'	AB3
Public Yard Frontage Type	up to 25'	AB4
Along all streets within 50' of a Residential Zone	prevailing <i>setback</i> of Residential Zone	C

Notes:

- For individual lot frontages on Major Streets, at least 50% of the lot frontage must have *building facade* at the *build-to line*.
- For individual lot frontages on non-Major Streets, at least 30% of the lot frontage must have *building facade* at the *build-to line*.
- Individual lot frontages must have *building facade* at the *build-to line* within 30' of street intersections involving a Major Street.

Perimeter Yard Minimums ¹		
Along Street Frontages	0'	D
Street frontages within 50' of a Residential Zone	Prevailing <i>setback</i> of Residential Zone	C
On Lot Interior:		
abutting Residential Zone	25' except within Jefferson Park, which would be NR-1	E
abutting non-Residential Zone	0'	F
lots abutting alley may use one-half alley width toward <i>perimeter yard</i> requirement		G

Frontage Types	
Porch	Allowed
Public Yard	Allowed
Private Yard	Allowed
Courtyard	Allowed
Parking Lot	Allowed but only for 80 linear feet of frontage at a time and cannot be within 30' of an intersection involving a Major Street
Park or Plaza	Allowed

Notes:

- See Frontage Types Section 4.5.5 for standards for each Frontage Type.
- Vehicular drives prohibited along Major Streets within 30' of intersections.
- Use of Frontage Types must also achieve *build-to line* requirements, however some Frontage Types adjust the location of the required *build-to line*; see *Build-to Line* section.
- Publicly accessible open spaces should be treated as non-Major Streets for the purposes of allowed Frontage Types.
- Active ground floor uses are required on 80% of *building facade* on all streets and open space frontages. Active uses include retail space, lobbies, entries, offices, living rooms, and kitchens of residential uses, common rooms and recreation spaces of institutional and public gathering uses, and other similarly active uses. These do not include storage areas, bathrooms, bedrooms, or other similarly less active or private uses.

¹ See Frontage Types in Section 5.5.7 for Perimeter Yard Maximums

Segment Central

Building Height, Massing & Articulation

Note:

For properties within the Jefferson Park Overlay, match the requirements of the Neighborhood Design Standards for Jefferson Park NR-1 district. For areas outside of Jefferson Park, the height requirements of the existing underlying zone apply.



Segment Central

Permitted Land Uses

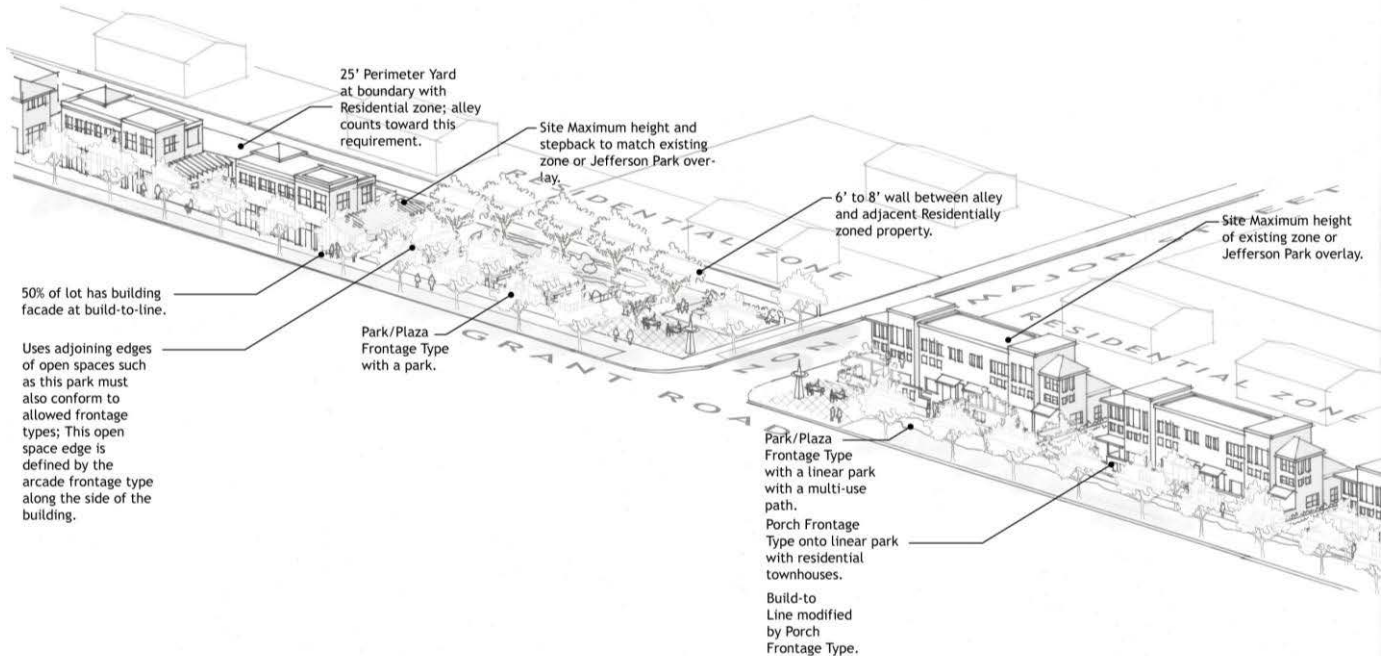
Note:

Land uses per existing underlying zone.

Segment Central

Illustrative View

One possible scenario showing how new and modified existing development could meet the development standards of the Segment Central category.





Segment Central

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4. General Zoning Standards

4. GENERAL ZONING STANDARDS

4.1 Purpose

The general standards applicable to all zoning categories in the Grant Road District (GRD) are provided in this chapter order to avoid repetition between the standards for specific categories, and to provide an understanding of the broad goals and intent of the zoning district.

4.2 Applicability

The General Zoning Standards are applicable to all Categories within the Grant Road District.

4.3 Use Types and Mixing of Uses

The Grant Road District uses the Land Use Groups and Land Use Classes as defined in the Tucson LUC, dated April 4, 2011. See Article VI. Definitions. Division 3. Land Use Groups of the Tucson LUC for definitions. The uses allowed within the Grant Road District's zoning categories are described in the standards for each specific category.

While non-residential zoning districts in Tucson generally provide for a mix of commercial and residential uses, this has not resulted in the type of vibrant, walkable environment that the community would like to see along Grant Road. The Grant Road District is intended to support the creation of a mixed use pattern of development that meets the community's goals.

This is done by allowing a broad range of uses within most of the Grant Road District zoning categories, similar to that allowed by existing commercial zones. Some of the categories allow a broader mix of uses with performance standards to mitigate potential negative impacts from noise, dust, etc. The zoning allows for a mix of uses within each Center and Segment as well as within individual properties. There is also a focus on providing a buffer and transition between the GRD and adjacent single-family neighborhoods. In order to achieve the desired mix of uses and character of development, the GRD allows for and supports both horizontal and vertical mixing of uses.

4. Grant District Categories

Horizontal Mixed Use distributes uses side-by-side and maximizes access to uses by creating a compact and walkable environment, see Figure 4.1.a. Uses may be arranged along Grant Road where lots are shallower, and may also transition across larger properties with uses arranged around a courtyard, plaza or *pedestrian pass-through*. Horizontal mixing of uses can also serve to mitigate impacts on adjacent single-family neighborhoods by locating lower impact uses and lower-scaled buildings closer to more sensitive uses and those with higher impacts away from them. For example, a project containing shops, restaurants and townhomes can locate the shops and restaurants along the busier street and transition into a quieter setting with townhomes adjacent to existing single-family residential uses. Similarly, a project with industrial or service commercial uses might locate those towards the street and locate small office spaces or live/work townhomes towards existing single-family residential uses.

Vertical Mixed Use stacks multiple, but compatible uses on the same parcel. The use at the ground floor is the most “public” and benefits from the most interaction with activity on the street and sidewalk, supporting the desired active environment along Grant Road, see Figure 4.1.b. These uses are often restaurants, shops, and offices. Uses above the ground floor typically desire greater privacy from an active sidewalk and street life. These are usually residences or office uses. When a *vertical mixed use* building is adjacent to a single-family neighborhood, the Grant Road District standards require a stepping down in the height of buildings closer to the adjacent neighborhood, and encourage complimentary uses such as offices, residences, open space, or parking in closer proximity to the neighborhood with landscaping and a wall nearest to the property line or alley.

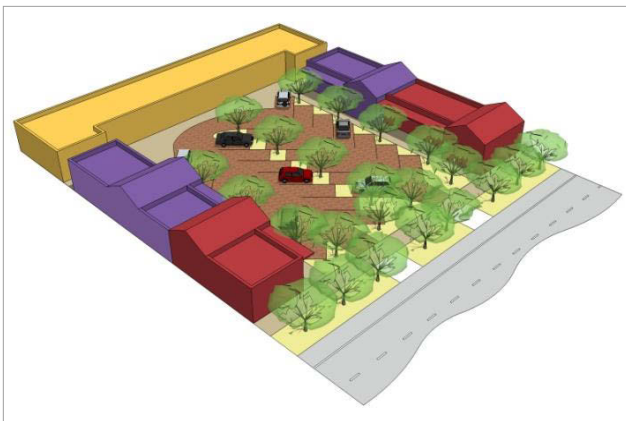


Figure 4.1.a: Horizontal Mixed Use Diagram showing non-residential uses at the street and residential uses at the rear.

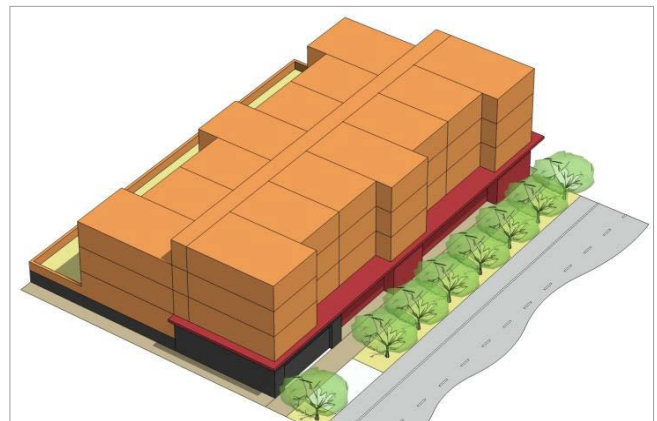


Figure 4.1.b: Vertical Mixed Use Diagram showing residences over retail.

4. General Zoning Standards

4.4 Block and Multi-Modal Circulation Standards

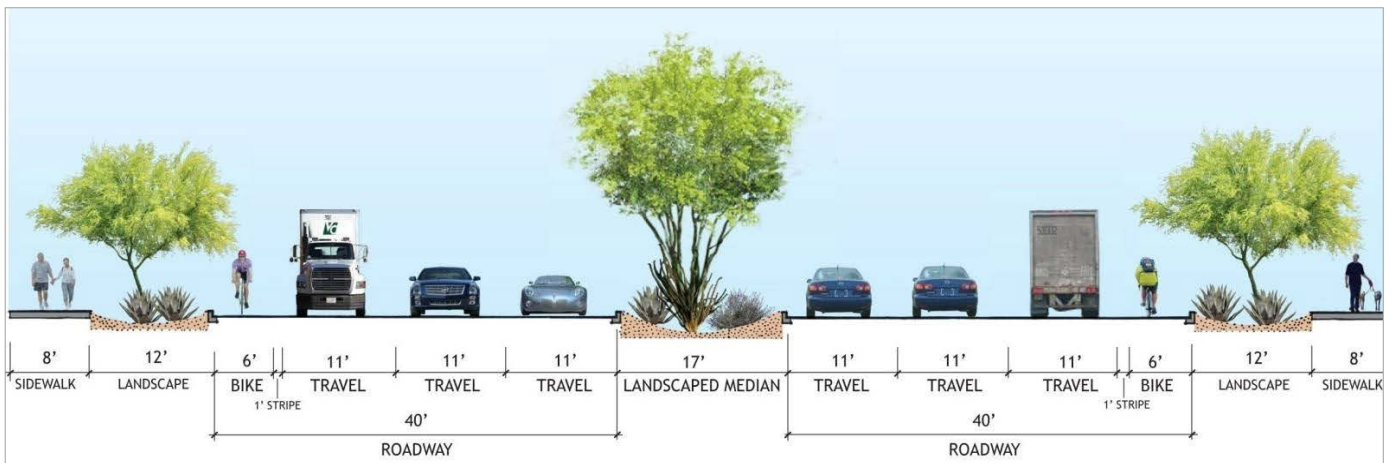
4.4.1 Purpose

A vibrant pedestrian and transit supportive environment, such as that desired along Grant Road, has a circulation network designed for all modes of travel (i.e., vehicles, trucks, bicycles, and pedestrians) with streets, alleys, pathways and other travel routes that best serve each mode; as well as meeting standards for disabled access, ADA. These standards support the creation of blocks, streets, and other bicycle and pedestrian circulation improvements within the Grant Road District to conveniently and safely serve all modes in order to complement the pedestrian- and bicyclist-supportive redesign of Grant Road itself. See Figures 4.4.1.a and 4.4.1.b, which illustrate the redesigned Grant Road.

4.4.2 Applicability

These block and street standards apply to all new development located within the Grant Road District (GRD) where new streets are proposed and where existing streets do not meet the sidewalk and other improvement standards of the GRD.

The following street standards are applicable only for the pedestrian realm of the street, the area between the curb and the back of sidewalk, or back of landscape strip in certain cases, for collectors or local streets. Arterials perpendicular to Grant Road shall apply the Grant Road design within the pedestrian realm (*sidewalk area*). See section 3.2.13 of the Tucson Land Use Code and 3-01.0 of the Tucson Development Standards for roadway (curb-to-curb) areas and right-of-way standards for these streets (as accessed November 2011).



Figures 4.4.1.a and 4.4.1.b: Typical cross section of Grant Road Improvements and simulation of improvements at a bus stop.

4. Grant District Categories

4.4.3 Block Standards

Purpose

Block standards for the Grant Road District are provided to guide new development, on larger sites in the GRD, in creating a pedestrian-friendly street network that provides pedestrians, bicyclists and drivers with easy access to destinations. An interconnected street network is essential to encouraging alternative modes of travel. Blocks that are compact reduce the real and perceived distance of walking for pedestrians, and more frequent intersections also reduce the repetitiveness of frontages and increase the amenities and destinations that are within a walkable range. Figure 4.4.3.a is illustrative of how a larger site along Grant Road could be transformed from a shopping center with large surface parking lots into a mixed center with a network of streets and blocks.

Standards

The following block standards give dimension of allowable street block sizes, expressed in feet of width and length for rectilinear blocks and perimeter circumference for irregular blocks.

- Block length per side: 500' max. linear feet
- Block perimeter length: 1,700' max.
- Block lengths and perimeters greater the standards defined herein shall break up the block with a pedestrian/bicycle access such as a pass-through that offers a direct connection to both ends of the block. See Section 4.4.4. Street Standards for pass-through standards.

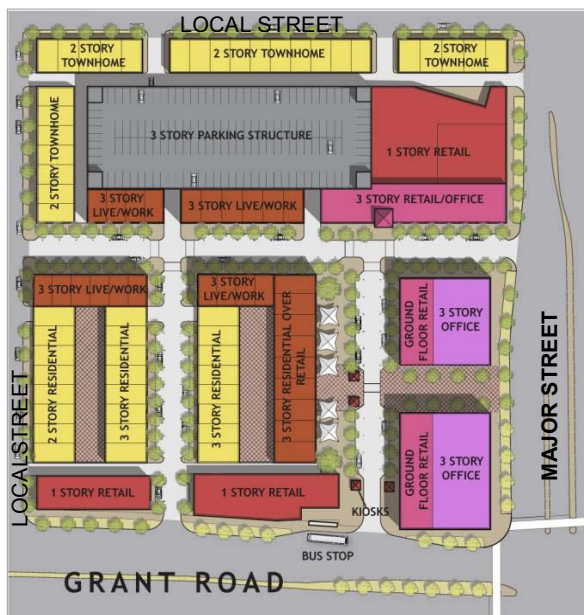


Figure 4.4.3.a: Diagrammatic plan of before and after showing how an existing shopping center might redevelop using the GRD, illustrating how new streets create blocks sized to meet the design standards.

4. General Zoning Standards

4.4.4 Street Standards

Purpose

The following street standard dimensions are established for local (new and existing) and collector streets (for arterials see the Sidewalk Areas subheading in this section) to:

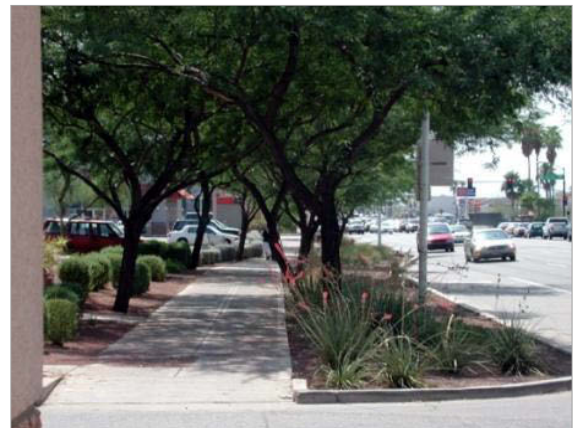
- Ensure adequate, comfortable, and attractive pedestrian realm along streets within the Grant Road District;
- Provide appropriate areas for landscaping to create a comfortable and appealing pedestrian realm that is attractively landscaped and shaded, see Figure 4.4.4.a;
- Create buffers between pedestrians and the roadway or parking lane and provide buffering for adjacent development where greater privacy or separation from the *public realm* is needed, see Figures 4.4.4.b and 4.4.4.c;
- Provide areas for water harvesting which can also serve as attractive landscape elements; and,
- Ensure that adequate space is provided for occupants of vehicles that are parked at the street curb to enter and exit.

These objectives further the standards as stated in the LUC 3.2.8. In particular:

- Increase public safety by minimizing the conflict between vehicular and pedestrian activities;
- Improve air quality in the City of Tucson by providing design standards for pedestrian circulation that support walking and transit as alternative modes of transportation; and,
- Provide for streets of suitable location, width, and improvement to safely accommodate vehicular, pedestrian, disabled pedestrian, and bicycle traffic.



Figure 4.4.4.a: Photo of an example sidewalk pedestrian realm that is comfortable and appealing for pedestrians.



Figures 4.4.4.b and 4.4.4.c: Examples of buffering between the pedestrian realm and the roadway, a photo of a built example and a simulation of the Grant Road Improvements.

4. Grant District Categories

Standards

General Standards

Where the following Grant Road District standards vary from the Tucson Development Standards Section 3-01.1.0, the GRD standards shall prevail.

Street Composition: Tucson Development Standards Section 3-01.2.0, with the following exception:

- Existing on-street parking may not be reduced or eliminated, and new local streets must include parking on both sides of the street.

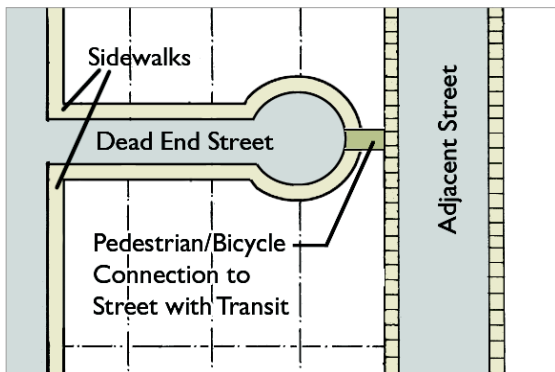


Figure 4.4.4.d: Diagram of a pedestrian-pass through connection between a dead end street and an adjacent street.

Street Layout: Tucson Development Standards Section 3-01.6.0 applies with the following exceptions:

- New local streets shall be arranged as an interconnected street grid.
- Permanent dead end streets shall not exceed the maximum defined block length, as per standards in the subheading for general standards in this section, of 500 feet without a *pedestrian pass-through*, see Figures 4.4.4.d and 4.4.4.e.

Intersection Sight Visibility: Per Tucson Development Standards Section 3-01.5.0.

Criteria for Cutting of Pavement: Per Tucson Development Standards Section 3-03.0.

Loading and Unloading Requirements reference LUC Section 3.4.4.1.A with the following exception:

- Loading and unloading may be accessed from a street abutting a residential zone provided that the loading area is at least 20 feet away from the zone boundary.



Figure 4.4.4.e: Photo of an example pedestrian pass-through.

4. General Zoning Standards

Alleys

Applicability

The following standards apply to Public Alleys.

Standards

- Twenty (20) foot right-of-way shall be standard for alleys within the Grant Road District.
- When a project uses an alley for parking or loading access, a design professional review is required.

Street Design Criteria: Tucson Development Standards Section 3-01.6.0 applies with the following exception:

- Alleys may be used as a primary point of access.
- If parking access is provided through an alley and the access would result in more than 30 peak hour trips and a residential use abuts any portion of the alley providing access to adjacent streets, the owner of the property implementing the GRD is responsible for ensuring that a six-foot high masonry wall is present between the alley and the residential use to minimize noise. If the owner of the residential property(ies) does not desire the wall, the requirement can be waived.

Street Improvements: Tucson Development Standards Section 3-01.3.0 applies with the following exceptions:

- Alleys shall be paved according to the standards for local streets.
- ❖ A 2-foot strip on either side may be enhanced pavement or special paving, including permeable paving such as brick pavers or other paving units on sand, permeable asphalt or concrete; and non-permeable surfaces such as stamped concrete, colored concrete, and paving units on a non-permeable base.

Solid Waste Disposal: Tucson Development Standards Section 6-01.2.1.B.1 shall apply.

Loading and Unloading Requirements: LUC Section 3.4.4.2.B shall apply with the following exceptions:

- Exception to LUC Section 3.4.4.2.B.1. The alley is used for access to only two (2) loading areas for sites larger than 20,000 square feet.
- Loading and unloading may utilize an alley when adjacent to a residential zone.
- If an alley is used for maneuvering of delivery vehicles and a residential use abuts any portion of the alley providing access to adjacent streets, the owner of the property implementing the GRD is responsible for ensuring that a six-foot high masonry wall is present between the alley and the residential use to minimize noise. If the owner of the residential property(ies) does not desire the wall, the requirement can be waived.

The following additional exceptions apply to Private Alleys:

- Private Alleys are permitted to be primary vehicular access to any lot.
- Are not required to be signed or striped.

4. Grant District Categories



Figure 4.4.4.f: Photo of example sidewalk improvements on a collector street with a Public Storefront Frontage Type.



Figure 4.4.4.g: Photo of example sidewalk improvements on a collector street with a Parking Lot Frontage Type.



Figure 4.4.4.h: Simulation sidewalk improvements along the improved Grant Road with a Public Storefront Frontage Type.

Sidewalk Areas

Purpose

A *sidewalk area* is the term used in the Tucson Development Standards and Land Use Code for the area that “generally consists of a space between the curb and sidewalk, area for a sidewalk, and a ground slope area. Improvements include, but are not limited to, wheelchair ramps, sidewalks, driveways, utilities, street furniture, landscaping, and drainage structures.” (Development Code Section 3-01.2.7). In the Grant Road District, the *sidewalk area* typically consists of an area for landscaping, either within a strip or with tree well cutouts, a through sidewalk width for pedestrian travel along the sidewalk, and a “buffer” area along the edge of the sidewalk or adjacent building. See Figures 4.4.4.f through 4.4.4.h. In some cases, the buffer area is wide enough for outdoor seating or display of commercial goods, and in some conditions a landscape strip is provided between the back of walk and an adjacent wall, building, or parking facility.

Standards

- All streets require a *sidewalk area* on each side of the street.
- Reduction in the width of the *sidewalk area* below the minimum standards defined in Table 4.4.4.a is not allowed.

Tucson Development Standards Section 3-01.0 applies to the Grant Road District with the following exceptions:

- Sidewalk improvements meeting the minimum standards defined in Table 4.4.4.a are required for all new construction, and improvements to existing buildings and site development (other than cosmetic improvements or reconstruction of damaged improvements).

Existing sidewalks shall transition either on or adjacent to the project site to match sidewalk standards provided in Table 4.4.4.a. Off-site improvements require evidence of agreement with adjacent property owner(s).

4. General Zoning Standards

Table 4.4.4.a: Pedestrian Realm Standards for Collector and Local Streets¹

Frontage Type	General Center, West Segment, and East Segment					Central Segment			
	Collector w/o Parking	Collector w/ Parking and Water Harvesting Basin	Collector w/ Parking ²	Local w/ Parking and Water Harvesting Basin	Local w/ Parking ²	Collector w/o Parking	Collector w/ Parking and Water Harvesting Basin	Collector w/ Parking ²	Local w/ Parking ²
Public Storefront	TYPE 1 16' total 10' landscape w/ trees 6' sidewalk	TYPE 3 16' total 2' parking access walk 8' landscape w/ trees 6' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk	15'-6" total 1'-6" parking access walk 8' landscape w/ trees 6' sidewalk	TYPES 1 OR 2 12' total 6' for trees w/ grates or landscaped tree wells 6' sidewalk	TYPE 1 16' total 10' landscape w/ trees 6' sidewalk	TYPE 3 15'-6" total 1'-6" parking access walk 8' landscape w/ trees 6' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk	TYPE 1 14' total 8' landscape w/ trees 6' sidewalk
Private Storefront			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip					TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip	
Arcade			TYPE 2 14' total 6' for trees w/ grates 8' sidewalk						
Porch			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip					TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip	
Service Garage			TYPE 2 14' total 6' for trees w/ grates 8' sidewalk						
Public Yard			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip					TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip	
Private Yard			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip					TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip	
Courtyard with Public Storefront, Arcade, Service Garage, Public Yard, or Park Plaza frontage			TYPE 2 14' total 6' for trees w/ grates 8' sidewalk					TYPE 2 14' total 6' for trees w/ grates 8' sidewalk	
Courtyard with Private Storefront, Porch, or Private Yard frontage			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip					TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip	
Shared Motorcourt with Public Storefront, Arcade, Service Garage, Public Yard, or Park Plaza frontage			TYPE 2 14' total 6' for trees w/ grates 8' sidewalk						
Shared Motorcourt with Private Storefront, Porch, or Private Yard frontage			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip						
Parking Lot			TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip						
Park/Plaza			TYPE 2 14' total 6' for trees w/ grates 8' sidewalk					TYPE 2 14' total 6' for trees w/ grates 8' sidewalk*	



4. Grant District Categories

Table 4.4.4.a: continued

Frontage Type	Marketplace, Crossroads, and Hub Centers					Neighborhood Centers				
	Collector w/o Parking	Collector w/ Parking and Water Harvesting Basin	Collector w/ Parking ²	Local w/ Parking and Water Harvesting Basin	Local w/ Parking ²	Collector w/o Parking	Collector w/ Parking and Water Harvesting Basin	Collector w/ Parking ²	Local w/ Parking and Water Harvesting Basin	Local w/ Parking ²
Public Storefront	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 2 16' total 6' for trees w/ grates 10' sidewalk				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk		
Private Storefront	TYPE 4 18' total 10' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 5 21' total 3' parking access walk 8' landscape w/ trees 8' sidewalk 2' landscape strip	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip				TYPE 5 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip		
Arcade	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 2 16' total 6' for trees w/ grates 10' sidewalk				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk		
Porch	TYPE 4 18' total 10' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 5 21' total 3' parking access walk 8' landscape w/ trees 8' sidewalk 2' landscape strip	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip				TYPE 5 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip		
Service Garage	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 2 16' total 6' for trees w/ grates 10' sidewalk				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk		
Public Yard	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip		
Private Yard	TYPE 4 18' total 10' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 5 21' total 3' parking access walk 8' landscape w/ trees 8' sidewalk 2' landscape strip	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip	TYPE 3 16'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk	TYPES 1 OR 2 13' total 6' for trees w/ grates or landscaped tree wells 7' sidewalk	TYPE 1 16' total 10' landscape w/ trees 6' sidewalk	TYPE 5 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip	TYPE 3 15'-6" total 1'-6" parking access walk 8' landscape w/ trees 6' sidewalk	TYPES 1 OR 2 12' total 6' for trees w/ grates or landscaped tree wells 6' sidewalk
Courtyard with Public Storefront, Arcade, Service Garage, Public Yard, or Park Plaza frontage	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 2 16' total 6' for trees w/ grates 10' sidewalk				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk		
Courtyard with Private Storefront, Porch, or Private Yard frontage	TYPE 4 18' total 10' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 5 21' total 3' parking access walk 8' landscape w/ trees 8' sidewalk 2' landscape strip	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip				TYPE 5 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip		
Shared Motorcourt with Public Storefront, Arcade, Service Garage, Public Yard, or Park Plaza frontage	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 2 16' total 6' for trees w/ grates 10' sidewalk				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk		
Shared Motorcourt with Private Storefront, Porch, or Private Yard frontage	TYPE 4 18' total 10' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 5 21' total 3' parking access walk 8' landscape w/ trees 8' sidewalk 2' landscape strip	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip				TYPE 5 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip		
Parking Lot	TYPE 4 18' total 10' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 6 16' total 6' for trees w/ grates 8' sidewalk 2' landscape strip				TYPE 5 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 6' sidewalk 2' landscape strip	TYPE 6 14' total 6' for trees w/ grates 6' sidewalk 2' landscape strip		
Park/Plaza	TYPE 1 18' total 10' landscape w/ trees 8' sidewalk	TYPE 3 21' total 3' parking access walk 8' landscape w/ trees 10' sidewalk	TYPE 2 16' total 6' for trees w/ grates 10' sidewalk				TYPE 3 18'-6" total 2'-6" parking access walk 8' landscape w/ trees 8' sidewalk	TYPE 2 14' total 6' for trees w/ grates 8' sidewalk		

¹ Total dimensions do not include 6 inch vertical curb.

² Water harvesting can be accommodated with structural soils under sidewalk.

4. General Zoning Standards

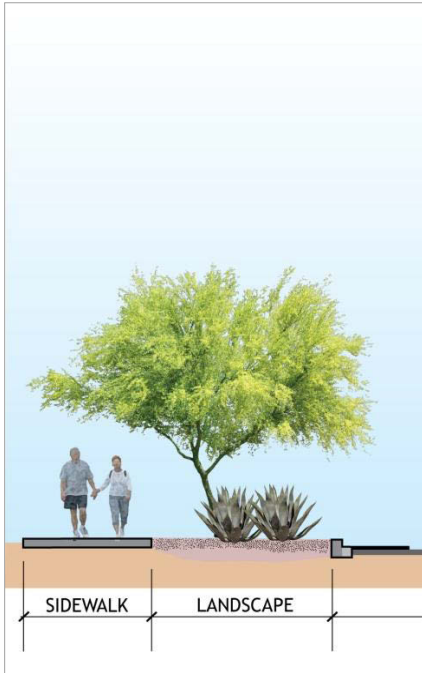


Figure 4.4.4.i: TYPE 1 - Sidewalk and landscape area with trees.



Figure 4.4.4.j: TYPE 2 - Sidewalk and trees in grates.

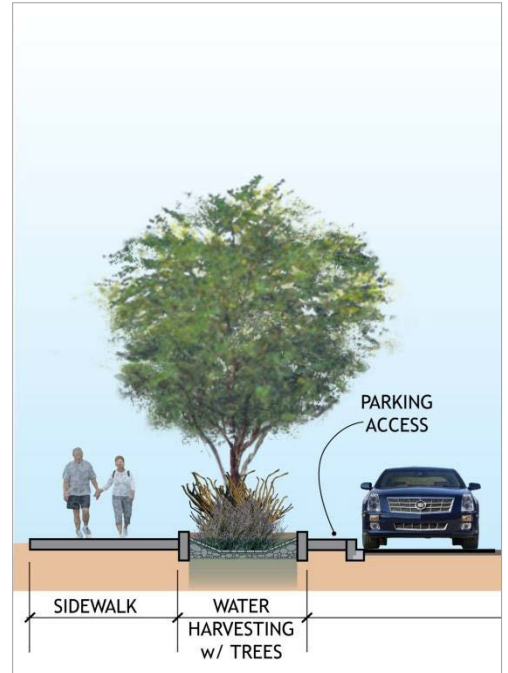


Figure 4.4.4.k: TYPE 3 - Sidewalk and water harvesting landscape with trees and paved parking access.

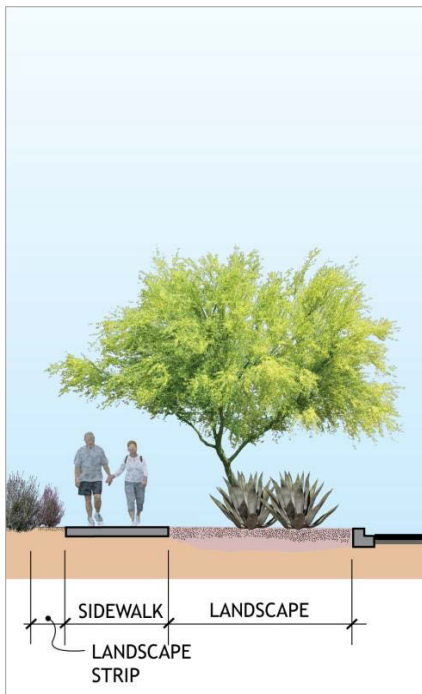


Figure 4.4.4.l: TYPE 4 - Landscape strip with sidewalk and landscape area with trees.



Figure 4.4.4.m: TYPE 5 - Landscape strip with sidewalk and trees in grates.

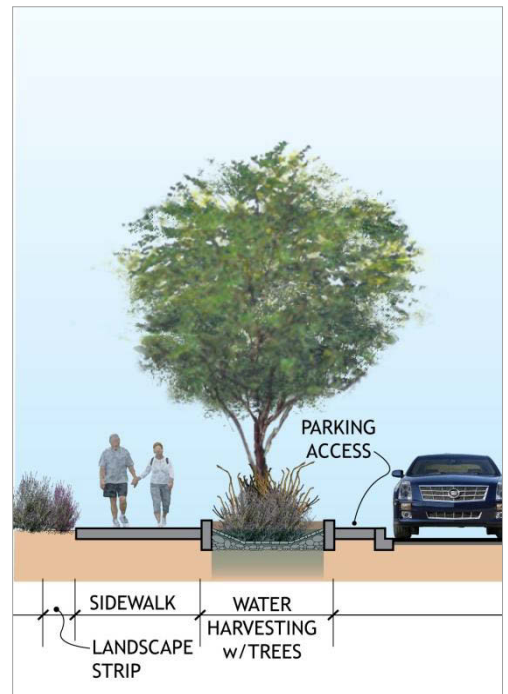
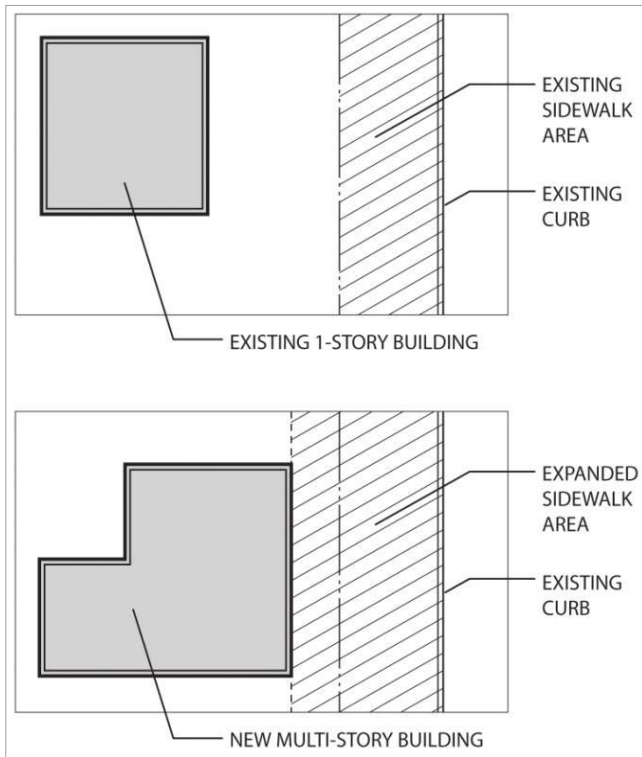


Figure 4.4.4.n: TYPE 6 - Landscape strip with sidewalk and water harvesting landscape with trees and paved parking access.

4. Grant District Categories



Figures 4.4.4.o and 4.4.4.p: Comparison of existing sidewalk area with Grant Road improvements and dedication/easement.

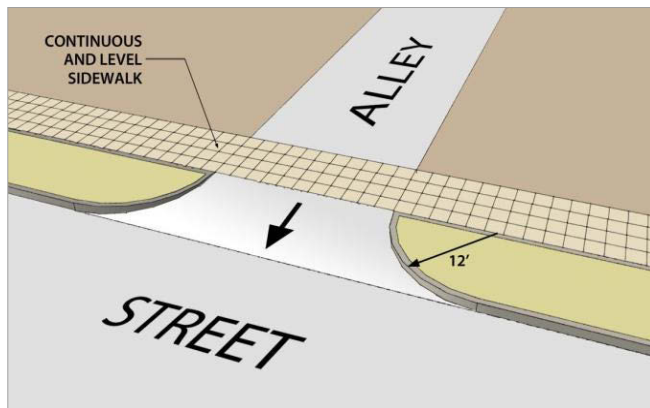


Figure 4.4.4.q: Diagram of GRD standards for a curb cut providing access to an alley.

- Streets within the Grant Road District shall include all elements defined in Table 4.4.4.a, with no exceptions for reductions to minimum widths or elimination of elements.
- In some cases the *sidewalk area* requirements of the Grant Road District will extend beyond existing street right-of-way, in which case the *sidewalk area* shall either be dedicated to the City or put into a public access easement. See Figures 4.4.4.o and 4.4.4.p.
- Table 4.4.4.a provides minimum widths for sidewalk elements for local and collector streets. Variations are based on level of expected pedestrian activity, allowed frontage types, and other conditions. See Figures 4.4.4.i through 4.4.4.n at left. Dimensions defined in this table supersede Illustration 3.2.8.4.A of the Development Standards.
- The design of the *sidewalk area* for arterial streets within the Grant Road District shall follow the Grant Road standard.

Tucson Development Standards Section 3-01.3.0 applies to the Grant Road District with the exception of the following:

- Vertical curbing is required on all streets. Wedge or rolled curbs are prohibited.
- At intersections of alleys with streets within the GRD a curb cut is the desired method for connecting the street and alley as this will allow for the through portion of the sidewalk, a 6 foot minimum width, to pass across the connection to the alley at roughly the same grade as the adjacent sidewalk. The returns of the curb cut shall have a minimum radius of twelve (12) feet and a maximum radius of fifteen (15) feet. If the alleys are not paved, the ends of the curb returns will be connected with a concrete header at the back of sidewalk, see Figure 4.4.4.q.
- Meandering sidewalks (Pedestrian Parkways) are prohibited.

4. General Zoning Standards

Construction Standards: Tucson Development Standards Section 3-01.4.0 applies to the Grant Road District with the exception of the following:

- ❖ **Ramp direction:** Curb ramps shall align in the direction of crosswalks, with two per corner at each intersection and at right angles to the curb, rather than having one “diagonal” curb ramp per corner. See Figures 4.4.4.r and 4.4.4.s.



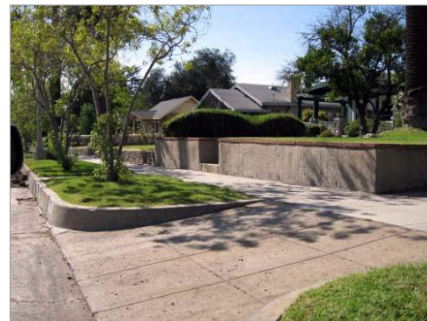
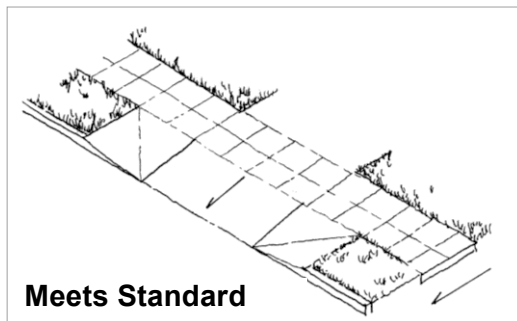
Figures 4.4.4.r and 4.4.4.s: Diagram and photo example of directional curb ramps.

4. Grant District Categories

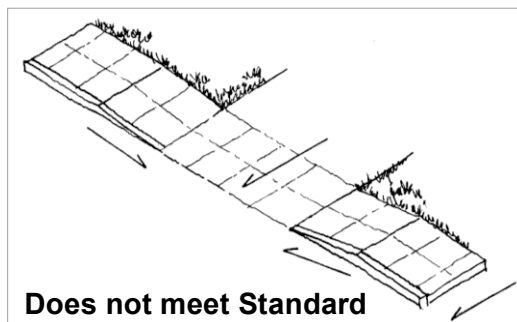
Driveways

Standards

- ❖ **Accessible Route Cross Slope:** Driveway aprons will be reconstructed so the sidewalk portion of the apron does not exceed two (2) percent cross slope. Sidewalks shall not meander around driveway aprons.
- ❖ Sidewalk grades, paving material, and design shall be maintained and continuous across driveways, curb cuts, and intersections with alleys. See Figures 4.4.4.t and 4.4.4.u.
- ❖ Dropped driveways are not allowed. See Figures 4.4.4.v and 4.4.4.w.
- See Section 4.7. Site Design for additional driveway and access standards.



Figures 4.4.4.t and 4.4.4.u: Diagram and photo example of continuous paving across driveway.



Figures 4.4.4.v and 4.4.4.w: Diagram and example photo of a dropped driveway condition that does not satisfy the GRD standards.

4. General Zoning Standards

Pedestrian Pass-throughs

Purpose

Pedestrian pass-throughs are public or private accessways, or pedestrian connections, that extend the *public realm* of the street sidewalk by allowing pedestrians access between buildings or through parcels where a local street is not feasible or desirable. See Figures 4.4.4.x through 4.4.4.z. Buildings may side-on to a *pedestrian pass-through*, although pedestrian amenities are provided to create an inviting and safe space. A *pedestrian pass-through* can be integrated into the open space of a Courtyard or Motorcourt frontage type.

Standards

- Pass-throughs are required where blocks cannot be maintained at less than 500 feet in length or where they are needed to provide a direct route to nearby schools or public parks.
- Pass-throughs shall be a ten (10) foot minimum width and shall be as straight as possible to improve sightlines and security. *Pedestrian pass-throughs* shall be visible from publicly accessible streets or open spaces, from at least one end of the pass-through, for safety.
- Minimum clear walkway width shall be six (6) feet wide.
- Pass-throughs, which are made available through public/private agreements shall be accessible between the hours of 6:00am and 10:00pm.
- ❖ Pass-throughs shall visually and physically connect pedestrians to meaningful destinations such as open spaces, streets, shops, building entries, and parking lots.

- ❖ Pass-throughs shall avoid terminating at the backs of buildings, loading areas, and storage and refuse areas.
- ❖ Pass-throughs shall provide ample lighting. See Section 4.8 Lighting for height and design for light standards and other lighting standards.
- ❖ Pass-throughs shall be landscaped and paved at a level of design and quality that encourages their use so that they do not become neglected or dangerous spaces. Landscape elements may include planters and/or planting beds, trellises, arbors, water features, murals, decorative paving, seating, art and decorative lighting. Decorative fences and walls are allowed.
- Fences and walls fronting onto pass-throughs may extend up to a maximum of eight (8) feet, with decorative screening elements, which may be up to a maximum of ten (10) feet.
- Decorative screening elements, such as arbors, lattices, or grill work that extend above eight (8) feet in height shall be a minimum of 20% visually permeable.
- ❖ Buildings that abut pass-throughs should orient uses that with some level of activity towards the pass-throughs. This might include day care facilities, conference rooms, office spaces, etc.
- Secondary entrances to *buildings* may be accessed from pass-throughs.
- ❖ Building walls and landscape walls siding onto pass-throughs shall be articulated and shall follow blank wall standards of the associated frontage type.



Figures 4.4.4.x through 4.4.4.z: Photo examples of pedestrian pass-throughs from other communities. The pass-through in Figure 4.4.4.x meets standards with the exception of minimum walkway width.

4. Grant District Categories



Figures 4.4.4.aa and 4.4.4.bb: Photo examples of passive water harvesting associated with street improvements in Tucson.

Water Harvesting in Streets

Purpose

If non-major streets within the Grant Road District are improved, the developer should incorporate rainwater harvesting into any improvements they make to the street and its pedestrian realm, utilizing a water harvesting approach that fits with the design intent of the street standards detailed in this section of the GRD. Rainwater harvesting can provide additional water to street trees and other landscaping; can remove harmful pollutants from runoff from storms; can mitigate floods; can store water for other uses; and can provide for attractive landscape and potentially *functional open space*. Rainwater harvesting elements range from passive swales, infiltration trenches and rain gardens that can detain, retain, and filter rainwater runoff to active underground cisterns that can store water for later reuse. Each one of these approaches may be appropriate for streets in the GRD depending on the size and design of the street and the adjacent uses.

There are an increasing number of good examples of passive water harvesting associated with street improvements, such as Scott Street in Downtown Tucson, see Figure 4.4.4.aa, the improvements around the pocket park in the Broadmoor-Broadway neighborhood south of Broadway, see Figure 4.4.4.bb, and the improvements to Grant Road will also create new examples as its phases are constructed.

Applicability

These standards apply to any public and private street and pedestrian realm within the Grant Road District.

Standards

- When a development makes improvements to an adjacent street or pedestrian realm that includes water harvesting features stormwater that is generated within the development's property can flow into the water harvesting features if a hydrologic study and stormwater management plan is prepared and approved by the Director.
- See Section 4.10 Environmental Resources for additional water harvesting standards.

4. General Zoning Standards



Figure 4.4.5.a: Simulation illustrating a chicane providing traffic calming and pedestrian amenities at an entry point between new development on Grant Road and an adjacent neighborhood.

4.4.5 Traffic Calming

Purpose

Traffic calming in the Grant Road District will serve to minimize potential traffic impacts resulting from development of properties in the GRD on adjacent residential and other existing development. The goal is to encourage drivers traveling to and from development along Grant Road and its centers and segments to use Grant Road or other major streets, to slow the development or residential traffic that enters an adjacent neighborhood, and to give identity to neighborhood entries as well as create opportunities for additional landscape and open space.

The necessity for traffic calming on local streets serving adjacent neighborhoods depends on a number of factors that can affect the amount of traffic generated along these streets. Should traffic calming be necessary, a narrowing of the roadway through a chicane or choker may be utilized to achieve the GRD's traffic calming goals.

Applicability

These standards apply to new development or redevelopment with at least one access directly onto a local street or with at least one access to an alley that connects to a local street, when the local street provides access to an adjacent existing residentially zoned area and if the new uses increase ADT on the local street beyond the ADT that the current uses of the site generate by 15% or more and there is an increase in trips of at least 100 during the a.m. or p.m. peak hour.

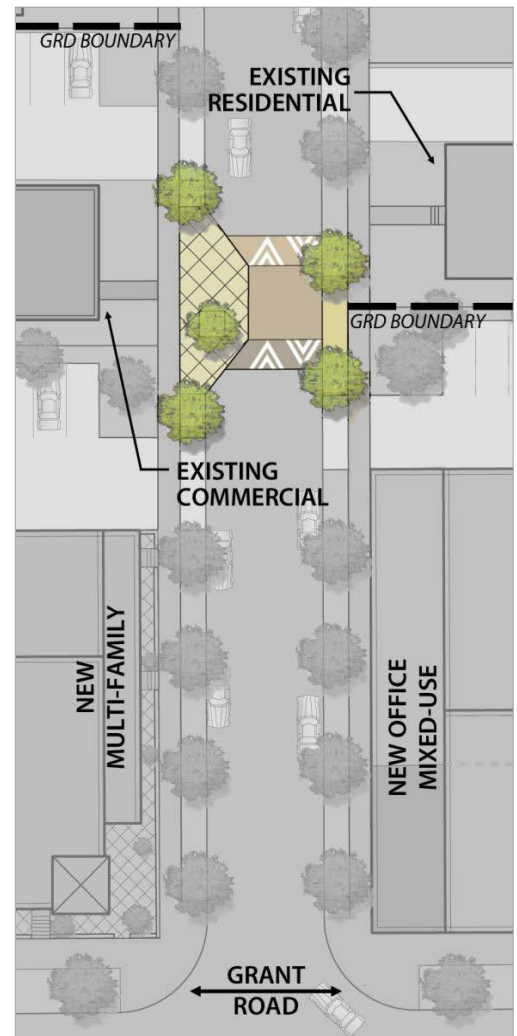


Figure 4.4.5.b: Diagram of a chicane located between driveways.

4. Grant District Categories



Figure 4.4.5.c: Diagram of a chicane and speed table at driveway where location restrictions exist.

Standards

- A chicane or choker with a speed table is the preferred traffic calming feature, see Figure 4.4.5.a.
- An alternative traffic calming feature may be used if approved by TDoT.
- The preferred location for a chicane is in proximity to the transition from the GRD zone to the adjacent residential neighborhood.
- Chicanes or chokers should be located between driveways. See Figure 4.4.5.b
- Where a chicane or choker must be located in front of a driveway it shall be designed to allow access from the driveway onto the speed table. See Figure 4.4.5.c.
- Chicanes shall extend out into the roadway to create a deflection in the roadway such that vehicles are prevented from taking a straight and unobstructed line through the chicane or they may create a yield lane where the roadway is narrowed to one lane for both directions, a choker.
- Signage shall be provided to indicate the travel condition (e.g., yield and/or speed table).
- The length and location of chicanes shall be determined based on an analysis of factors including the unique environment of the street, expected traffic volume, and desired speed reduction.
- The design of a chicane or choker shall be approved by TDoT, and property owners within 100 feet of the traffic calming improvement shall be notified and given the opportunity to comment on the design of the improvement.
- Chicanes shall provide additional improvements along the *sidewalk area* such as landscaping, stormwater management, or hardscape accompanied by seating and planters. Shade shall be provided with seating.
- ❖ Frontage conditions shall be taken into account in determining the character of the additional *sidewalk area*.

4. General Zoning Standards

4.5 Street Frontage and Building Placement Standards

4.5.1 Purpose

Street Frontage and Building Placement Standards address the ways in which buildings and the sites relate to streets and neighboring properties in the Grant Road District. The way in which sites “front” onto Grant Road, cross streets, other circulation networks, and public open spaces, and the way sites abut onto neighboring sites address major objectives of the Grant Road Community Character and Vitality Plan and the GRD. The *street frontage* of properties adjacent to Grant Road is a major contributor to the character of the street with the goal of being pedestrian supportive. Other site boundaries help define the relationship between the more urban and mixed use development along Grant Road and the residential neighborhoods behind it.

4.5.2 Applicability

The Grant Road District’s *Street Frontage* and Site Boundary Standards address a site’s perimeter differently than in the City of Tucson Land Use Code (largely addressed in Section 3.2.6). The GRD method for determining *perimeter yards* is applicable only to the GRD.

4.5.3 Perimeter Yard and Build-to Line (BTL)

Purpose

Perimeter yards and *build-to lines* regulate building placement in relation to a site boundary, helping to define *street frontage* and side and rear site boundaries.

Applicability

Perimeter yards and *build-to lines* are applicable to all parcels within the GRD. The specific standards for *perimeter yards* and *build-to lines* vary by GRD zoning category and can be found in Section 3 Grant Road District Categories.

Each lot or site must have building façade at the *build-to line* for a given minimum percentage of its frontage along a street, see Figure 4.5.3.a. This percentage depends on the GRD Category that applies to the parcel. The location of the *build-to line* also varies by GRD Category, and sometimes by frontage type. The percentage of each site’s *street frontage* that is needed to satisfy the minimum coverage requirement may be either at the *build-to line* or set back further into the site, and like all *street frontage* it must meet the other requirements of the frontage types, see Section 4.5.7 Screening.

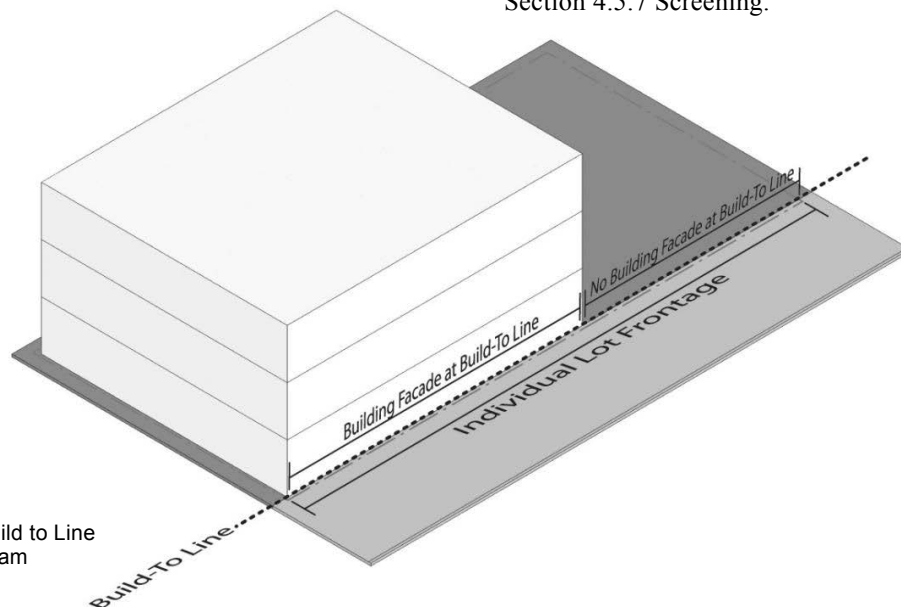


Figure 4.5.3.a: Build to Line Applicability diagram

4. General Zoning Standards

Measurement

Perimeter Yard: Per Land Use Code Section 3.2.6.3, with the following exception:

- Excluding the minimum requirement for *perimeter yard* to be 3 feet.

Build-to Line (BTL): A *build-to line* is measured by taking the horizontal distance from a) the exterior property line along Grant Road or b) the back of the *sidewalk area* on other streets, to the face of each exterior building wall at the wall's lowest point, see Figure 4.5.3.b. A vertical structural member (i.e., post, column, dormer wall) supporting a roof extension from the principal structure is considered a wall for applying *perimeter yard* and *build-to line* requirement

If the *build-to line* is more than 0 feet, that number signifies the maximum distance that the building façade can be back from the point of measurement along the street while still complying with the *build-to line* requirement.

In order to accommodate structural columns, building *articulation* at the façade, and other architectural elements, a building façade can jog back from the *build-to line* and still be counted as meeting the *build-to line* requirement as long as the variation is no more than 2 feet.

Setbacks: Setbacks are measured from the *build-to line*.

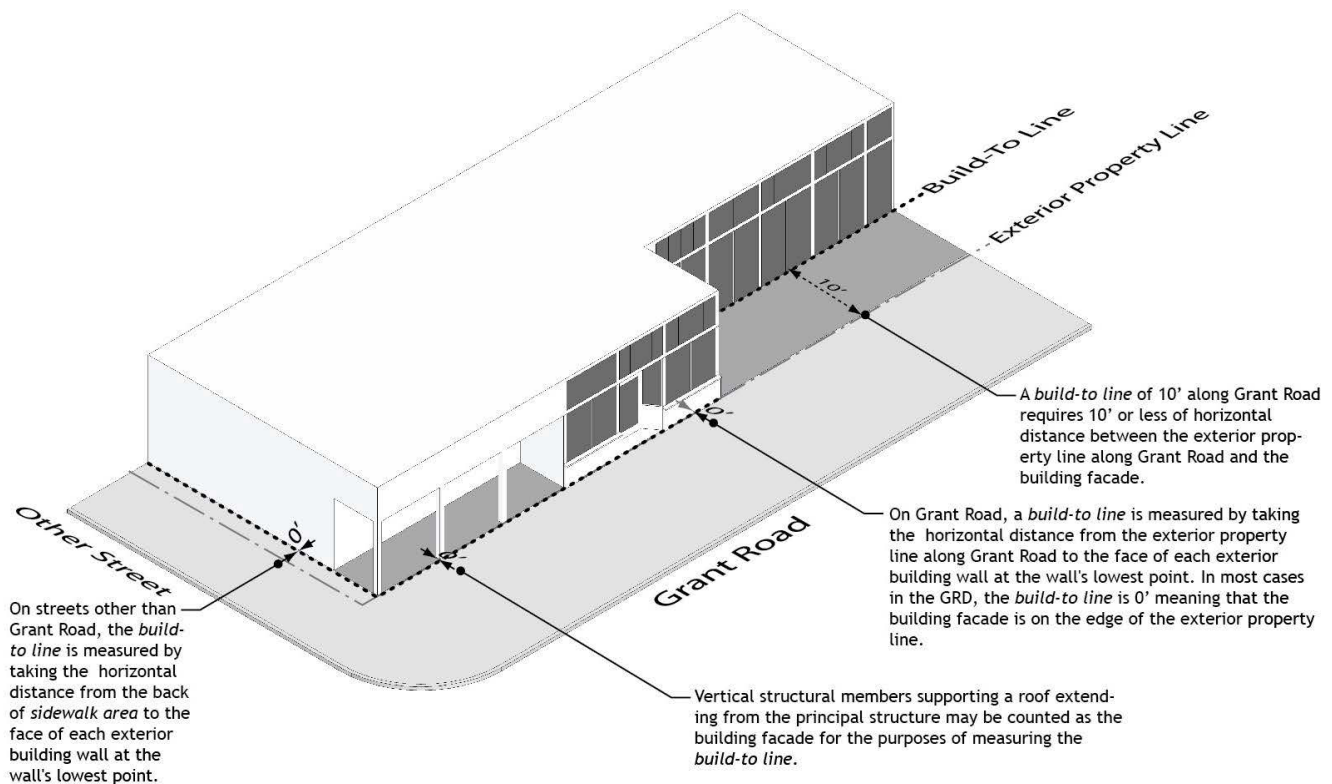


Figure 4.5.3.b: Build to Line measurement diagram

4. General Zoning Standards

Exceptions

A choice of frontage type can change the requirements for a *build-to line*, see Figure 4.5.3.c. For example, in some categories, choice of the Public Yard frontage type to occupy a portion of the *street frontage* allows for the *build-to line* to be up to 25 feet back from the right-of-way line. Choice of the Shared Motorcourt frontage type reduces the percentage of *individual lot frontage* required to have building façade at the *built to line*. See Section 3 Grant Road District Categories for specific standards.

Parks, Plazas, and Recessed Entries: Parks, plazas, and recessed entries may count toward the *build-to line* coverage requirement and the intersection corner requirement. Parks and plazas must conform to the Park or Plaza Frontage Type.

Curved Corner Frontage: Along *curved corner street frontage*, the *build-to line* can be achieved if the building façade is no more than 35% of the radius of the curve back from the point of measurement, see Figure 5.c in Section 5. Definitions.

District Parking: *District parking* lots are exempt from *build-to line* requirements. If a *district parking* lot is part of a development, the length of its *street frontage* may be removed from consideration of the *build-to line* coverage requirement.

Setbacks: *Setbacks* allow for additional area to widen a sidewalk to provide more space for pedestrians, active uses such as seating, dining and display, or landscaping where *vertical elements*, such as walls or fences, are present to allow for landscape planting to soften the edge.

Standards

Perimeter Yard: Per Land Use Code section 3.2.6.3, with the following exception:

- For an unbroken section of a wall or a building that is not parallel with the lot line, the required *perimeter yard* may be less than 3 feet.

For specific standards for perimeter yards and build-to lines, see dimensional standards in the diagrams and tables for each zoning category in Section 3 Grant Road District Categories.

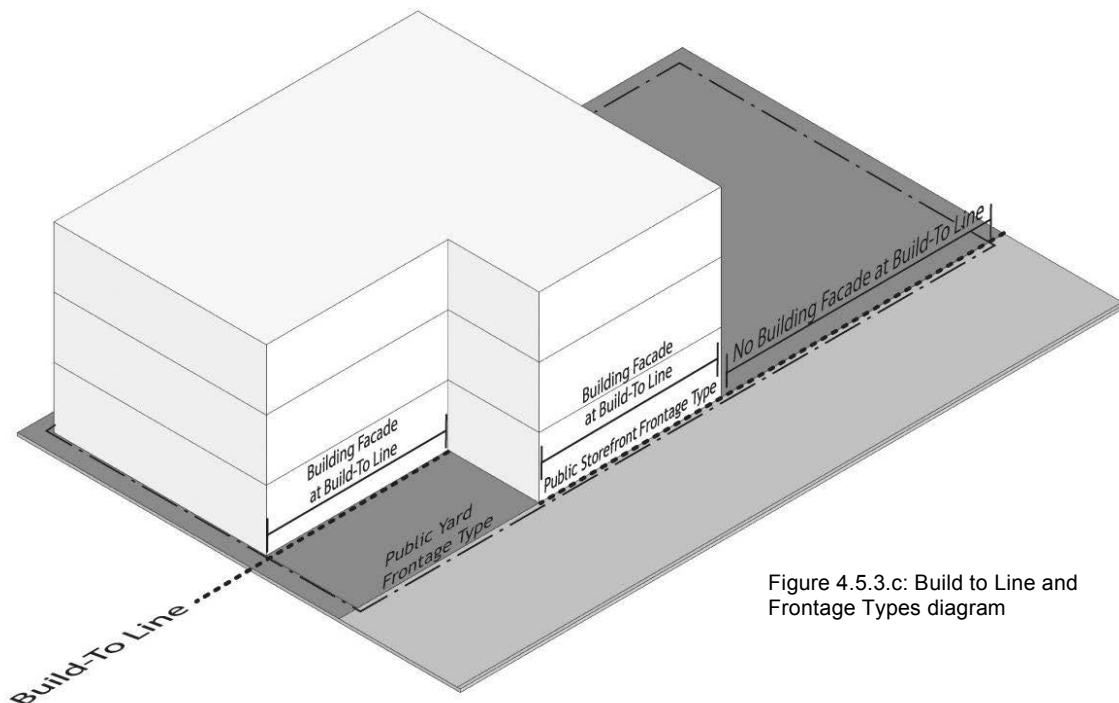


Figure 4.5.3.c: Build to Line and Frontage Types diagram



4. General Zoning Standards

4.5.4 Encroachments

Purpose

Encroachments define exceptions for building or landscape structures to project into areas that would otherwise prohibit private built elements. *Encroachments* allow greater flexibility for development to increase *building articulation* and create comfortable or active pedestrian spaces.

Standards

- Awnings, arbors and canopies may encroach into the *perimeter yard* up to 8 feet and into the public R.O.W. and publicly accessible open spaces up to 6 feet and must maintain a vertical clearance of 8 feet above the surface of sidewalks, pathways, or landscaped areas.
- Building eaves and bays may encroach into the *perimeter yard* or public right-of-way up to 6 feet, but must maintain required building and fire code separations at interior *perimeter yards*.
- The display of merchandise, dining facilities or seating may encroach into sidewalks, pass-throughs or publicly accessible open spaces such that a minimum 6' clear through zone is maintained for pass-throughs and 5' minimum for sidewalks.

4. General Zoning Standards

4.5.5 Frontage Types

Purpose

Grant Road frontage types help define the character of Grant Road by identifying configurations of buildings, yards, open spaces, and other built elements and how they front onto the streets adjacent to a site as well as other public and semi-public spaces. The frontage types cover the range of conditions that exist along Grant Road and the community's vision for the specific character of the Centers and Segments in the Grant Road District, whether built or unbuilt, public or private, pedestrian-focused or auto-accessible spaces. The unique list of types represented here should cover the range needed to complement the Grant Road design in creating a supportive "context" for the new multi-modal street.

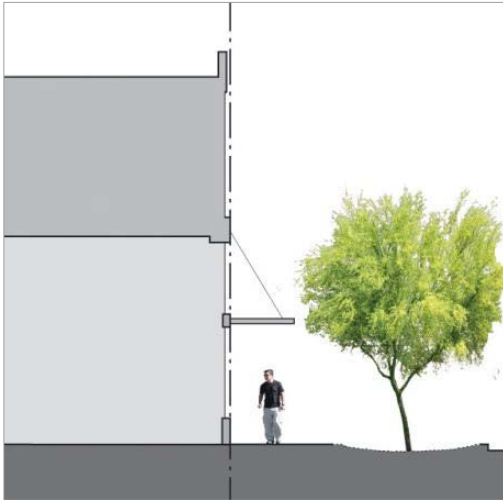
Frontage types define the street level portion of the building or space within a parcel that engages the *public realm* and *semi-public realm* of the street and open spaces along the street that are associated with some of the frontage types. Standards for the upper stories of buildings are defined in the Section 4.6.2 Building Articulation, and by the specific standards for the zoning categories within Section 3 Grant Road District Categories. Standards for the upper stories of buildings are defined in the Section 4.6.2 Building Articulation.

Applicability

All portions of *public frontage* for a given site must be occupied by one of the frontage types allowed for each Grant Road District Category or site access. Applicants must delineate all lengths of *public frontage* for their projects and note which allowed frontage type(s) apply along the *public frontage*. For a list of *frontage types* allowed under each Category, see Section 3 Grant Road District Categories.

- ❖ Note: A Design Professional is responsible for making the determination whether a frontage type that is claimed by an applicant conforms to the type as written in this section. In addition to the specific standards, the Design Professional should also determine whether the frontage fulfills the Purpose of the Frontage Type.

4. General Zoning Standards



Public Storefront

Purpose

The Public Storefront frontage type is a major basis for the character of the Grant Road Centers and is also a part of the Segments that link together the Centers along Grant Road. This type provides an open, welcoming face to the street and easy movement between the sidewalk and places of business or gathering. In the Public Storefront type, the building façade directly abuts the sidewalk and is characterized by frequent entrances and large areas of windows to provide visual *transparency* to the activity inside of the building. Enhanced detail and *articulation* in the façade design elements at the ground floor activate the *public realm*.

Standards

Windows

- ❖ **Window Area Minimum:** 75% of the length of street level building façade, as measured between 3' to 6'-8" above adjacent interior finish floor elevation.
- ❖ Includes windows, display windows, *transom windows*, and transparent doors.
- ❖ Opaque and reflective glazing and films are prohibited. Reflectivity shall not exceed 25% visual reflection for exterior of glazing system, and tinting shall not reduce *transparency* of standard glass by more than 35%. Low E or similar glazing treatments are preferred to minimize loss of *transparency*.
- ❖ **Obscuring of Glass:** 25% maximum by temporary signage. Interior shelving or display not meant for outdoor interest shall not exceed a maximum of 4' above finished floor elevation.

Entrances

- ❖ **Distance between Entries:** 50' max.
- ❖ **Recess:** 8' max.
- ❖ See the entrances portion of Section 4.6.2 Massing and Articulation for building entry design character standards.



4. General Zoning Standards

Blank Wall

- ❖ Maximum continuous horizontal length of blank wall along frontage façade of a building (i.e., the length of solid blank wall between windows or doors):
 - Grant Road and Other MS&R Arterial Streets: 12’
 - Other Streets: 18’
- ❖ Landscaping, art, signage, and/or other architectural elements shall be utilized to break up blank walls greater than 10’ in length.

Shading Elements

- ❖ Required on storefronts on MS&R for 75% min. length of building frontage; preferable in relation to *glazed area* of façade.
- ❖ Shading elements include: awnings, arbors, canopies, and similar architectural features.

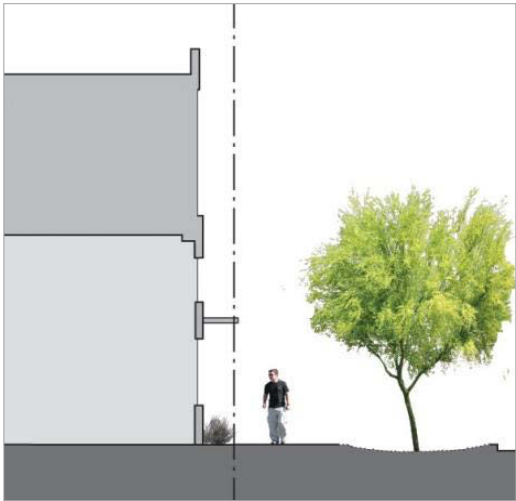
Encroachments

- ❖ **Clear Height over Sidewalk:** 8’ min.
- ❖ **Public Right of Way Overhang Encroachment:** 6’ max. No structure may come to grade within the public right of way.

Setback (to building)

- ❖ **Maximum Building Setback:** 3’
- ❖ Building *setback* shall be comprised of additional sidewalk to enhance the sidewalk environment by providing additional space for pedestrians, outdoor seating and dining, or display.
- ❖ See Section 3 Grant Road District Categories of the GRD for individual zoning category requirements for percentage of façade required at the build-to line (BTL).

4. General Zoning Standards



Private Storefront

Purpose

The Private Storefront frontage type is much like the Public Storefront, but it typically is employed by uses either not visited by the general public or those where visitation is appointment-based, such as professional or medical offices. The Private Storefront provides a measure or privacy for offices and other uses within buildings by allowing for some opacity of windows and the potential for a landscape screen to separate the façade from the sidewalk. But the Private Storefront also contributes to the Grant Road street environment by employing many of the aspects of the Public Storefront, such as direct or nearly direct frontage on the sidewalk, and a high ratio of glazing to solid wall.



Standards

Windows

- ❖ **Window Area Minimum:** 60% of the length of street level building façade, as measured between 3' to 6'-8" above adjacent interior finish floor elevation.
- ❖ Includes windows, display windows, *transom windows*, and transparent doors.
- ❖ Opaque and reflective glazing and films are prohibited. Reflectivity shall not exceed 35% visual reflection for exterior of glazing system, and tinting shall not reduce *transparency* of standard glass by more than 45%. Low E or similar glazing treatments are preferred to minimize loss of *transparency*.



Entrances

- ❖ **Distance between Entries:** 75' max.
- ❖ **Recess:** 8' max.
- ❖ See the entrances portion of Section 4.6.2 Massing and Articulation for building entry design character standards.

4. General Zoning Standards

Blank Wall

- ❖ Maximum continuous horizontal length of blank wall along frontage façade of a building (i.e., the length of solid blank wall between windows or doors):
 - Grant Road and Other MS&R Arterial Streets: 10’.
 - Other Streets: 16’.
- ❖ Landscaping, art, signage, and/or other architectural elements shall be utilized to break up blank walls greater than 10’ in length.

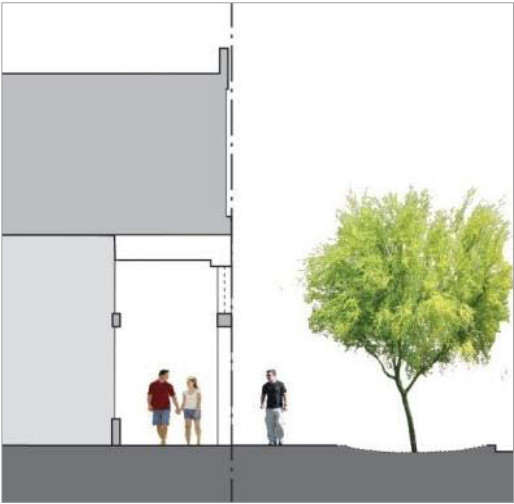
Shading Elements

- ❖ Required on storefronts on MS&R for 60% min. length of building frontage; preferable in relation to *glazed area* of façade.
- ❖ Shading elements include: awnings, arbors, and canopies.

Encroachments

- ❖ Clear Height over Sidewalk: 8’ min.
- ❖ **Public Right of Way Overhang Encroachment:** 6’ max. no structure may come to grade within the public right of way.

4. General Zoning Standards



Arcade

Purpose

The arcade frontage type is an open air space that is created by a colonnade supporting the upper floors of a building where the street-level façade of the building is set back. The arcade frontage provides protection from inclement weather and creates shade along the exterior of a building for pedestrians and interior uses, as such it is a particularly useful frontage for warmer climates like Tucson's. Arcades are accompanied with a set back public or private storefront frontage.

Standards

Dimensions

- ❖ **Minimum Height:** 12' clear.
- ❖ **Minimum Depth:** 8' clear.
- ❖ **Minimum Height of Arcade Opening:** 10'.

Design

- ❖ The arcade shall be treated as part of the main façade with similar architectural treatment.
- ❖ The width of arcade opening shall match the pattern of storefront windows and entrances.
- ❖ The height and the proportions of arcade elements shall be consistent with the proportions and style of the *building facade*.
- ❖ Balconies may be located above the arcade where upper stories are set back to create a gallery.
- ❖ Planters, signage, lighting, that are mounted to the arcade or building are allowed.

Shading Elements

- ❖ Shading elements that are mounted to the arcade or building are allowed.



4. General Zoning Standards

Encroachments

- ❖ Any projections into the arcade shall maintain a minimum clear height of 8' and minimum 6' width clear through space.
- ❖ Display of goods, dining, seating, signage, lighting and other pedestrian amenities are allowed within the arcade space as long as a minimum 6' width clear through space is maintained.
- ❖ **Right-of-Way Encroachments:**
 - Arcade shall be located solely within private property and shall not project into the right-of-way.

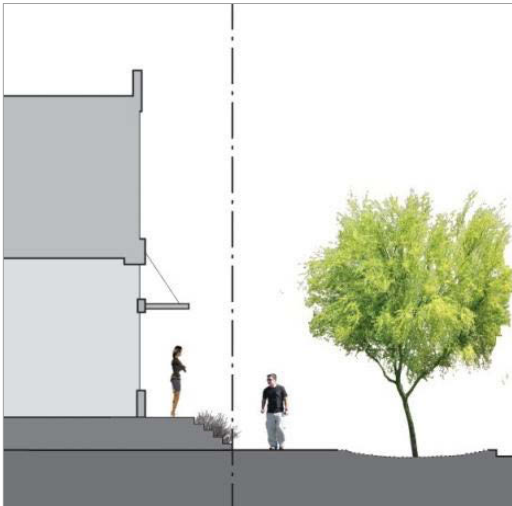
Paving

- ❖ Special paving is encouraged within the arcade space; at a minimum the paving shall meet City of Tucson public sidewalk standards.

Frontage Types

- ❖ The Arcade shall be combined with the Private or Public Storefront Frontage Types. The Private or Public Storefront facade is set back at the ground floor and shall follow standards for those frontage types.

4. General Zoning Standards



Porch

Purpose

The Porch frontage type accommodates living or working spaces needing a degree of privacy from the street but that still front onto it and engage the sidewalk. It provides privacy by setting back the main façade of the building and, in some cases, elevating the main floor, with a set of stairs and small yard in front of the façade. The porch creates the transition zone between the public space of the sidewalk and the private space of the building. It creates an inviting entry that offers protection from the elements.

Standards

Dimensions

❖ Porch Height

- Floor Elevation Above Sidewalk: 6" min. and 3' max.
- Height: 1 story max. for the porch space, but may have balcony above.

❖ Porch Clear Area

- Height: 8' min. clear.
- Depth: 6' min. clear.
- Width: 8' min. clear.



Design

- ❖ Porches shall have a roof for shading and weather protection; a balcony may be located above a porch to satisfy this requirement.
- ❖ Porches should be designed to be consistent with the proportions, form, character and style of the main *building facade*.

Windows

- ❖ Window Area Minimum: 50% min. length of street level building façade.



4. General Zoning Standards

Entrances

- ❖ **Distance between Entries:** 60' max. or at least one entry per unit.
- ❖ Each porch shall be connected to the sidewalk by a direct pedestrian pathway.
- ❖ See the entrances portion of Section 4.6.2 Massing and Articulation for building entry design character standards.

Blank Wall

- ❖ Maximum continuous horizontal length of blank wall along building façade (i.e., the length of solid blank wall between windows or doors):
 - Grant Road and other MS&R Streets: 20'.
 - Other Streets: 20'.

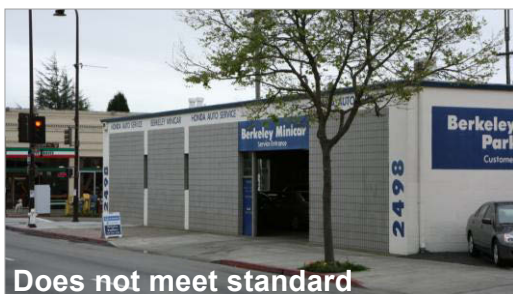
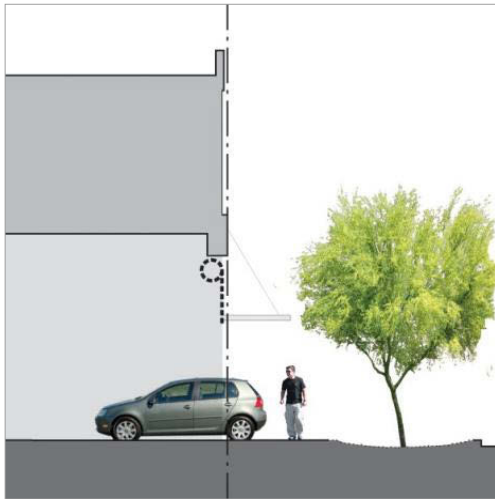
Landscape

- ❖ The yard between the *sidewalk area* and the building shall be landscaped and include a combination of trees, shrubs, groundcovers, and/or landscape structures.
- ❖ **Vertical Delineation:** a *vertical element* such as a wall, or fence may be located within the *setback* to delineate the front yard.
 - Maximum Height of Fences or Walls: 8'
 - Elements above 4' shall maintain a minimum 60% *visual permeability*.
 - Setback to Vertical Element: A 2' minimum deep landscaped area shall be provided between the *sidewalk area* and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% of the landscape *setback* not including the circulation space that directly connects the porch to the *sidewalk area*.

Frontage Types

- ❖ Porches may be combined with the Public and Private Yard, Courtyard, and Shared Motorcourt Frontage Types at the *building facade*. Building facades shall follow standards for those frontage types.

4. General Zoning Standards



Service Garage

Purpose

The façade of the Service Garage frontage type includes roll-up door entrances but must also include a primary pedestrian entrance to the building and a high degree of window coverage; primarily used for auto commercial services, light industrial, wholesale distribution, live/work, and similar business operations.

Standards

Windows

- ❖ **Main Façade Glazing:** 60% min. length of street level façade as measured between 3' to 6'-8" above adjacent interior finish floor elevation (excluding the garage door width from this calculation).
- ❖ Glazed area may be transparent or translucent.
- ❖ Extensive glazed garage doors are encouraged.

Entrances

- ❖ **Width of Garage Opening:** 24' max.
- ❖ **Door Recess:** 1' max.
- ❖ **Garage Door Glazing:** 20% min.
- ❖ Maximum one garage opening per *street frontage*.
- ❖ Minimum one pedestrian entrance per *street frontage*.

Blank Wall

- ❖ Maximum continuous horizontal length of blank wall along frontage façade of a building (i.e., the length of solid blank wall between windows or doors):
 - Grant Road and Other MS&R Streets: 10'.
 - Other Streets: 16'.
- ❖ Landscaping, art, signage, and/or other architectural elements shall be utilized to break up blank walls greater than 10' in length.

4. General Zoning Standards

Shading Elements

- ❖ Required on facade fronting along MS&R.
- ❖ Required on storefronts along MS&R for 60% min. length of building frontage (including the garage door width for this calculation).

Encroachments

- ❖ **Clear Height over Sidewalk:** 8' min.
- ❖ **Public Right of Way Overhang Encroachment:** 6' max. no structure may come to grade within the public right of way.

4. General Zoning Standards



Public Yard

Purpose

The Public Yard accommodates many of the outdoor activities that already take place on Grant Road, including eating, gathering, and display of commercial goods, but not parking. These activities can animate the street environment and provide pedestrian interest, especially if given the proper space for an engaging relationship with the street, and so this frontage type is an important component of Centers and Segments. The Public Yard is an outdoor space in front or at the side of a building that is open to patrons or residents, and which may also be accessible to the public. It also can be used for semi-public residential front yards. It should be delineated from the sidewalk, but easily visible and accessible.

Standards

Dimensions

- ❖ The dimensions of the yard should conform to the site design requirements mandated by each zoning category of the Grant Road District.

Landscape

- ❖ The public yard shall be delineated from the sidewalk by a change in material or a vertical delineation. This can be done in several ways, including:
 - Change in Material: Planting, decorative hardscaping, gravel, and/or other permeable surface. Asphalt is prohibited except for drive accessing parking area or service area beyond the public yard.
 - Vertical delineation: wall, seat wall, fence, landscape buffer, planters, entry gateway, and/or arbors.
 - Maximum Height of Fences and Walls: 8’.
 - Elements above 4’ shall maintain a minimum 60% *visual permeability*.
 - Chainlink and razor wire are not allowed.
 - Low walls may be used as integral seating.



4. General Zoning Standards

- Setback to vertical element: A 2’ minimum deep landscaped area shall be provided between the *sidewalk area* and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% of the landscape *setback* not including the circulation space that directly connects the yard to the *sidewalk area*.
- ❖ For maximum distance from building see individual zone categories under *Build-to Line*.

Frontage Types

- ❖ 100% of a Public Yard’s “Yard Frontage” shall be occupied by any combination of the frontage types allowed on the adjacent Street Frontage or a side or rear property line (see Figure 4.5.5.a).

Notes

- ❖ Yard Frontage is comprised of all edges of the yard except for those that are Street Frontage.
- ❖ The yard shall not be used for automobile parking or any other vehicular use.
- ❖ **Yard Minimum Occupiable Space:**
 - Residential Use: 10%.
 - Non-residential Use: 50%.

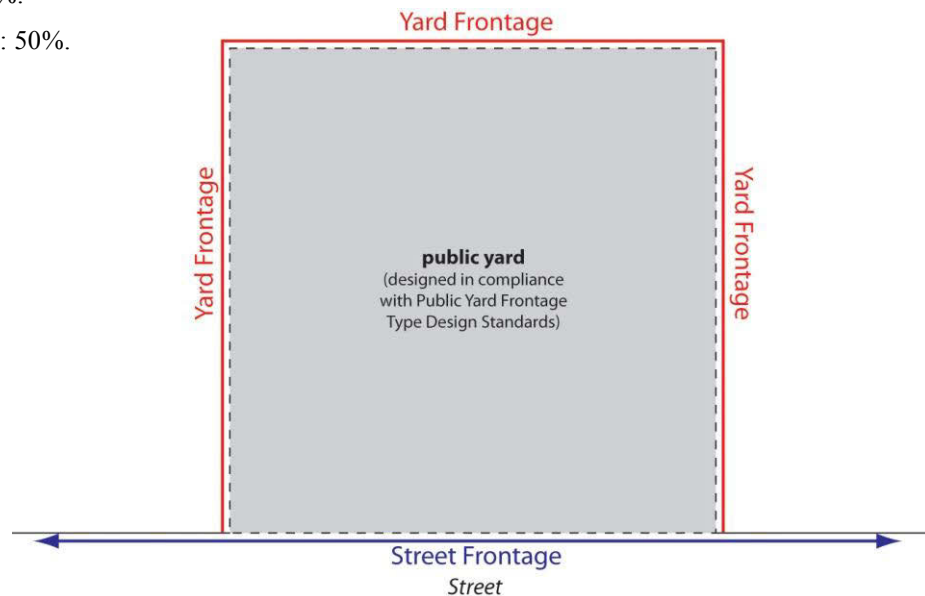
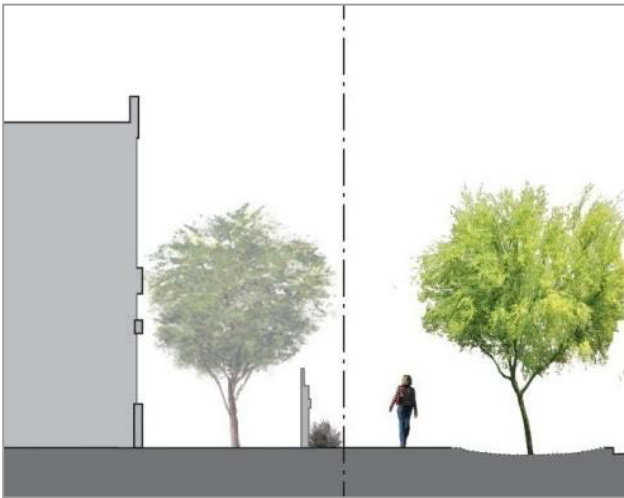


Figure 4.5.5.a: Yard Frontage diagram

4. General Zoning Standards



Private Yard

Purpose

The Private Yard accommodates outdoor space with a definition of privacy between the yard and the public street or open space, yet still balances privacy with animation of the sidewalk environment. The building and a tall wall along the perimeter of the front yard define the Private Yard to create a private or semi-private, outdoor room; this is a historic Tucson building form. Towards the street, this wall serves as the street façade along the sidewalk in lieu of a building. This configuration has the potential to eliminate the semi-public zone that provides a welcoming transition between the sidewalk and the building, reducing the sense of presence along the street. Also, whereas interior building activity can be sensed and provide a level of presence or “eyes on the street”, front yards typically see less activity. As a result, the frontage of the Private Yard must contain a high level of *articulation* and detail to create an engaging and dynamic frontage.

Standards

Entrances

- ❖ Entries into the yard shall be detailed to complement the design of the vertical screen.

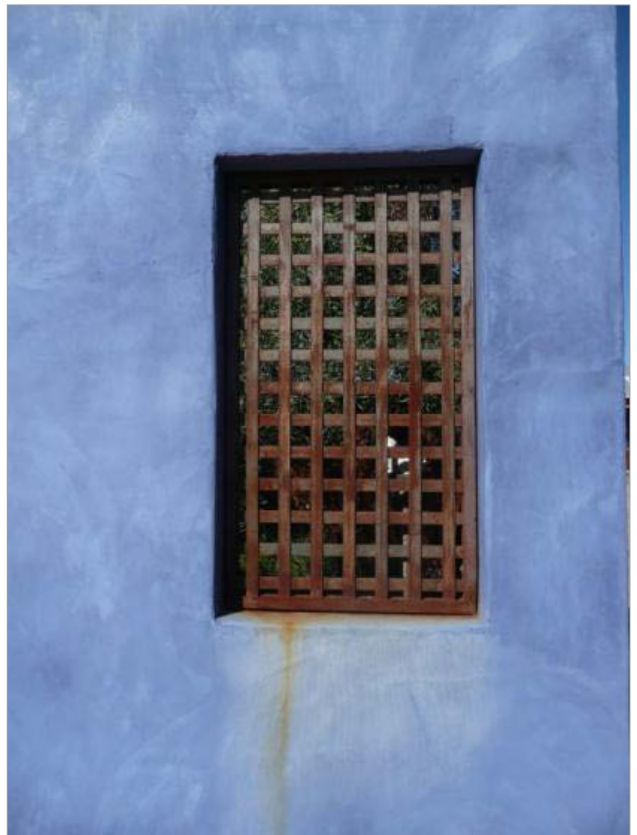
Blank Wall

- ❖ **Maximum Continuous Horizontal Length of Blank Wall:** 10' along streets.
- ❖ Lengths of wall greater than 10' shall be modulated or provide relief by a combination of the following means: change in horizontal profile (i.e., jogs in plane), material, architectural detail or other feature that breaks up the wall with complex texture, depth, and interest.
- ❖ The use of openings in the fence or wall at eye-level should be provided to allow for some visual connection between the yard and the sidewalk. These could be screened or grilled for security. Some *visual permeability* in the design of access gates is particularly desirable. See figures at right. Grills or screens shall provide at least 40% *visual permeability*.

4. General Zoning Standards

Landscape

- ❖ **Vertical Delineation:** The Private Yard shall be vertically delineated from the *sidewalk area*.
 - The private yard is delineated from the sidewalk by a substantial vertical built element. This can be done in several ways, including:
 - Walls, fences or raised planters.
 - Materials may include concrete, adobe, stucco, block and/or metal.
 - Chainlink and razor wire is not allowed.
 - Maximum Height of Fences and Walls: 8'.
 - Elements above 6' shall maintain a minimum 40% *visual permeability*.
 - Vertical elements shall not be the sole means of delineation and shall be accompanied by a landscape palette of a combination of trees, shrubs, cacti, and/or groundcovers, in the space between the *sidewalk area* and the *vertical element*.
 - Setback to vertical element: A 2' to 6' deep landscaped area shall be provided between the *sidewalk area* and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% of the landscape *setback* not including the circulation space that directly connects the yard to the *sidewalk area*.



Notes

- ❖ For corner lots:
 - All faces of a wall shall be treated similarly in detail.
 - Walls shall not obstruct vehicular sightlines.

4. General Zoning Standards



Courtyard

Purpose

The Courtyard Frontage Type is a combination of building and yard frontage that draws from Tucson’s architectural heritage and responds to Tucson’s climate. The frontage onto the adjacent street has two components: *building facades* and, between them, an internal open space, which is referred to as a courtyard. Building façades side or front onto the sidewalk (directly or with a small yard), and bracket a courtyard that is either separated from the sidewalk by a high wall, low wall or fence, landscape, or is open to the street. The configuration essentially creates a “U” shape as the buildings wrap around the courtyard space. Interior, main entries to individual units, storefronts, or lobbies front onto the courtyard. The courtyard is a shared common area for the uses surrounding it.

Because of its U-shaped configuration, this type differs from other frontage types (with the exception of the Motorcourt type) in that it is defined by multiple “public” or “semi-public” frontages: those that are located directly adjacent to streets (Street Frontage) as well as those frontages that surround the courtyard’s internal space (Courtyard Frontage). Because these frontages act as secondary transition zones between the public and private realm, these standards are defined for both public and internal courtyard frontages.

4. General Zoning Standards

Standards

Dimensions

- ❖ **Courtyard (open area):**
 - Minimum: 1:1.6 ratio of courtyard width (the dimension parallel to the *street frontage*) to building height or 18’ whichever is larger, and the depth (the dimension perpendicular to the *street frontage*) shall be at least 20’.
 - Maximum: 3:1 ratio of courtyard width to building height or 60’ whichever is less, there is no standard for maximum depth of a courtyard.
- ❖ **Minimum Building Facade at Courtyard Frontage:** 80%, See Figure 4.5.5.b.

Entrances

- ❖ A minimum 5’ walkway shall connect the sidewalk along the street that the courtyard fronts onto, to internal courtyard entries and the courtyard space.
- ❖ Primary entrances to buildings shall be directly accessed from the courtyard or the street.
- ❖ Minimum courtyard entry spacing shall comply with the associated frontage type for the buildings fronting onto the courtyard.

Landscape

- ❖ The courtyard shall be open to the sky.
- ❖ **Vertical Delineation:** The courtyard shall be vertically delineated from the sidewalk.
 - Vertical delineation can include walls, seat walls, fences, landscape buffers, planters and/or arbors.
 - Maximum height of fences and walls: 6’.
 - Elements above 4’ shall maintain a minimum 60% *visual permeability*.
 - Gates shall maintain a minimum of 60% *visual permeability*.
 - Chainlink and razor wire are not allowed.
 - Low walls may be used as integral seating.
 - Setback to Vertical Element: A 2’ to 4’ deep landscaped area shall be provided between the *sidewalk area* and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% of the landscape *setback* not including the circulation space that directly connects the courtyard to the *sidewalk area*.

Frontage Types

- ❖ 100% of the Courtyard Frontage shall be occupied by any combination of the frontage types allowed on the adjacent *street frontage*. See Figure 4.5.5.b.

Notes

- ❖ Courtyard Frontage is comprised of all edges of the Courtyard except for those that are Street Frontage.
- ❖ As part of a development using courtyard frontage types, a “half” courtyard can be developed where the courtyard is fronted by a ‘L’ of buildings with the courtyard space having two *street frontages*.

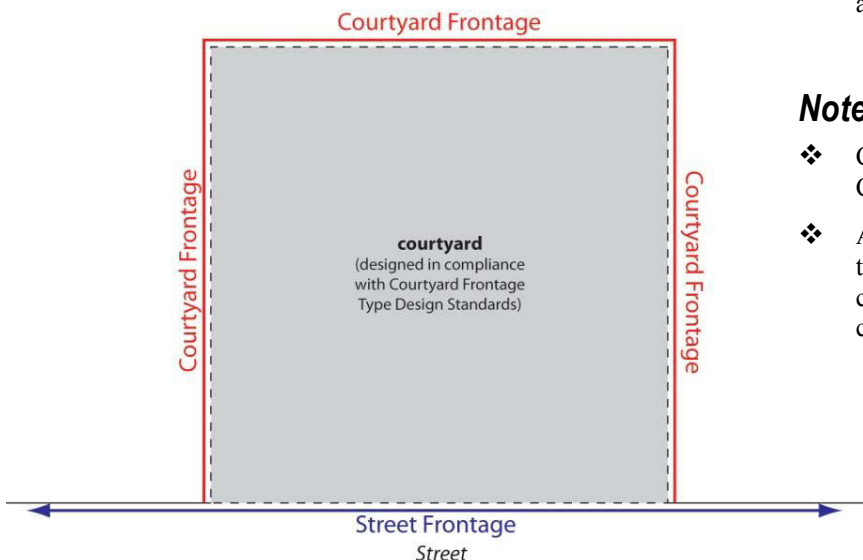


Figure 4.5.5.b: Courtyard (open area) Frontage diagram

4. General Zoning Standards



Shared Motorcourt

Purpose

Like the Courtyard, the Shared Motorcourt is a combination of different frontages that relate to Tucson’s architectural history and respond to Tucson’s climate. This frontage type draws from the historic motorcourt motels along Oracle Road, while at the same time addressing the future of Grant Road by creating space that can be “shared” by autos, pedestrians, bicycles and the disabled while allowing for a range and mix of uses.

This frontage type also accommodates the desire for parking “in front” of buildings while maintaining a lively, walkable *street frontage*. A number of design standards and recommended design treatments ensure that the Motorcourt is different from a typical strip commercial center.

The motorcourt (open area portion of this type) may contain a combination of an open space exclusively for pedestrians, parking and drive aisles, or it may take on the character of a shared plaza space that is designed primarily as a pedestrian space, but allows auto parking and circulation of vehicles at a slow and safe speed. Spaces in the motorcourt that are restricted to pedestrians should be differentiated from auto circulation for safety in ways that create attractive pedestrian spaces, such as color and material changes. Like in a courtyard, a motorcourt must contain trees or other landscaping to provide shade.

4. General Zoning Standards

Standards

Dimensions

- ❖ **Width Parallel to the *Street Frontage*:**
 - Minimum: 80’.
 - Maximum: 125’.
- ❖ Minimum *Building Facade* at Motorcourt Frontage: 80%, see Figure 4.5.5.c.

Entrances

- ❖ The buildings surrounding the motorcourt must have primary entrances onto the motorcourt or the street.
 - Minimum motorcourt entry spacing shall comply with the associated frontage type for the buildings fronting onto the motorcourt.

Landscape

- ❖ The motorcourt shall be open to the sky.
- ❖ A 10’ wide minimum pedestrian space (not shared with vehicles) shall line building frontage onto the motorcourt. This pedestrian circulation space shall connect to the *sidewalk area*.
- ❖ The pedestrian circulation around the perimeter of the motorcourt shall connect with the sidewalk.

A minimum 10% of the total motorcourt space, not including the perimeter pedestrian circulation space, shall be designed to be occupiable yard space where autos are prohibited.

- Elements to delineate spaces can include changes in paving, low walls or bollards, landscaping, or changes in elevation.
- ❖ At least 50% of the Motorcourt Frontage shall be landscaped and all frontage not used for auto and pedestrian circulation shall be landscaped.
- ❖ **Vertical delineation:** The motorcourt shall be vertically delineated from the *sidewalk area*.
 - Vertical Delineation can include walls, seat walls, fences, landscape buffers, planters and/or arbors.
 - Maximum height of fences and walls: 8’.
 - Elements above 3’ in height shall maintain a minimum 80% *visual permeability*.
 - Gates shall maintain a minimum 60% *visual permeability*.
 - Chainlink and razor wire are not allowed.
 - Low walls may be used as integral seating.
 - Setback to vertical element: A 2’ to 4’ deep landscaped area shall be provided between the sidewalk and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% or the landscape *setback* not including the circulation space that directly connects the motorcourt to the *sidewalk area*.

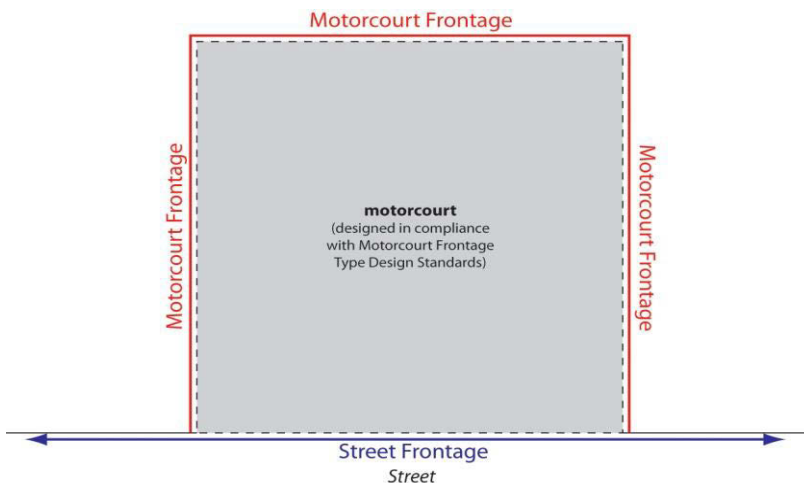


Figure 4.5.5.c: Motorcourt (open area) Frontage diagram



4. General Zoning Standards

Paving

- ❖ Hardscape space in the motorcourt used for vehicular circulation can be “shared” between cars, pedestrians, bicycles, and the disabled, but clearly delineated.
- ❖ The majority of motorcourt paved surfaces shall be decorative paving comprised of decorative concrete (i.e., decorative scoring patterns, and varied concrete colors and surface treatments), decorative pavers, or a combination of these to create a plaza environment. The use of permeable paving is desirable.
- ❖ Drive aisles may be paved with asphalt. The use of permeable paving is desirable.

Frontage Types

- ❖ 100% of Motorcourt Frontage shall be occupied by any combination of the frontage types allowed on the adjacent Street Frontage. See Figure 4.5.5.c.

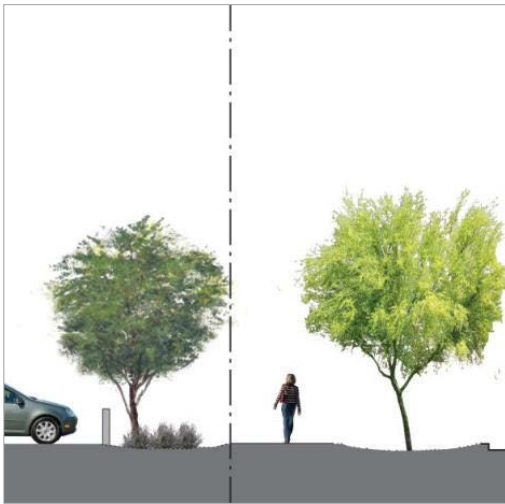
Notes

- ❖ Motorcourt Frontage is comprised of all edges of the motorcourt except for those that are Street Frontage.
- ❖ Motorcourts are not allowed on corner parcels and shall be a minimum of 50 feet away from any intersection.

4. General Zoning Standards

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4. General Zoning Standards



Parking Lot

Purpose

The Parking Lot is an important frontage type on Grant Road. At the same time, parking lots should not take away from the pedestrian supportive environment that is being created in the Grant Road District. The corridor's businesses and residences need accessible, convenient parking, and while much of the off-street surface parking needed for Grant Road is encouraged to be located at the back of lots, some parking will occur at the side of buildings. For the purposes of the frontage type, "parking lots" also include loading areas and outdoor service areas. Also, as larger shopping center sites evolve with incremental development over time, larger areas of surface parking will likely remain during these transition periods.

Auto and truck parking lots, and loading and service areas fronting onto streets and public spaces shall be effectively screened to reduce their visual presence. It is paramount that these frontages be well-designed to have a minimal detriment to the pedestrian environment of the streets in the Grant Road District. The Parking Lot frontage type must be buffered from the sidewalk with a substantial and varied landscaped area, and may include walls, fences, or trellis while allowing for adequate visibility into the lot for safety purposes. This buffer replaces the active frontage that a building can provide during the day and into the night, therefore it must strive to create a similar, high level of interest and complexity for pedestrians.



Standards

Blank Wall

- ❖ **Maximum Continuous Horizontal Length of Blank Wall:** 20' along streets or public spaces.
- ❖ Lengths of wall greater than 20' in length shall be modulated or provide relief through a change in vertical and/or horizontal profile (i.e. jogs in plane or silhouette), or may include an opening with at least 30% *visual permeability*.
- ❖ Portion of wall greater than 20' in length screening truck, service and storage areas shall also include trees to break up the mass.



4. General Zoning Standards

Shading Elements

- ❖ Shading elements may include trees, arbors, and/or other structures, such as tensile shade structures.

Landscape

- ❖ A horizontal buffer and vertical screening shall be used to create separation, provide interest along the street, and buffer streets and public spaces from vehicle headlights.
- ❖ **Minimum Buffer Width:** A 6' minimum buffer width shall be maintained to separate the parking lot from the *sidewalk area*.
- ❖ The buffer shall include a combination of shrubs and groundcovers with walls and/or fences. Trees may be included to provide added shade along the sidewalk and parked cars.
- ❖ **Vertical Delineation:** The parking lot shall be vertically delineated from the *sidewalk area*.
 - Trees shall be planted in a row with spacing such that their canopies touch when the trees are at 8-years maturity.
 - Understory landscaping shall be 4' minimum height.
 - Fences and walls shall be 4' min. and 8' max. height above parking lot.

- Elements above 4' shall maintain a minimum of 30% *visual permeability*.
- Setback to Vertical Element: A 2' to 4' deep landscaped area shall be provided between the *sidewalk area* and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% of the landscape *setback* not including the circulation space that directly connects the parking lot to the *sidewalk area* and driveways.

Notes

- ❖ Standards are applicable to all types of Parking Lots.
- ❖ A portion of the landscape *setback* can be hardscaped for pedestrians to access a food cart or similar use within the parking lot adjacent to the buffer.
 - Hardscaped area shall not exceed the length of a parking stall.
- ❖ Primary use by semi-trucks is a special condition only allowed in the Oracle Center and the Segment West zoning category.

4. General Zoning Standards



Park or Plaza

Purpose

The community has expressed a strong desire for additional community gathering places along Grant Road. The Park and Plaza frontage type can satisfy this desire while activating the street and improving the pedestrian environment. A park or plaza is an open space area actively used by the public for recreation, gathering, or relaxation. Parks and plazas can contain hardscape and landscape, but need to address water harvesting and stormwater management in a creative way that complements the urban landscape. Spaces should provide a balance of shaded and sunny areas, as well as protection from wind and rain, depending upon the orientation of the space, its use, and by season. It must be delineated from the sidewalk but easily visible, inviting, and accessible. Amenities should accommodate users of all ages as appropriate to surrounding uses.

Standards

Dimensions

- ❖ **Minimum Area:**
 - Plazas: 500 s.f.
 - Parks: 1000 s.f.
- ❖ **Minimum Dimension along *Street Frontage*:**
 - Plazas: 15 feet.
 - Parks: 20 feet.

4. General Zoning Standards

Encroachments

- ❖ Allowed *encroachments* include: dining facilities, seating, awnings, arcades, and signage.
- ❖ A minimum 8' clear space shall be maintained for all overhead structures.
- ❖ A minimum ADA path of travel shall be maintained as required for access.

Landscape

- ❖ Parks and plazas shall include a combination of landscaping, seating, lighting, water features, trash receptacles, decorative paving, low walls, lighting, public art, shaded areas (by means of landscape structures, trees or other methods) and visibly active spaces to provide visual interest to adjacent streets and activity that is visible from the street.
- ❖ The edges of Park or Plaza frontages shall be clearly delineated from the *sidewalk area* and shall include elements (e.g., landscaping, seating, decorative paving, gateways) to activate the street.
 - Park or Plaza frontages greater than 25 feet long shall have trees within 10 feet from the edge of the park or plaza and shall maintain one tree for every 25 linear feet of frontage.
 - Park or Plaza frontages shall include a decorative hardscape surface within 10 feet of the park or plaza edge for a 5' minimum width. Examples include decorative concrete, paver blocks, and crushed rock. Asphalt and non-decorative concrete do not count toward this requirement.
 - Park or Plaza frontages shall include seating, such as benches or seat walls, within 10 feet of the edge of the park or plaza.
- ❖ **Vertical Delineation:** A *vertical element* may be used to delineate the park or plaza from the *sidewalk area* and must meet the following requirements:
 - Vertical elements can include walls, seat walls, fences, landscape buffers, planters and/or arbors.
 - Walls and fences adjacent to the sidewalk shall be 6' max. height.

- Elements above 3' shall maintain a minimum 80% *visual permeability* in order to maintain a visually open frontage and a strong relationship to the street.
- In no case shall a park's or plaza's perimeter be bermed other than as infrequent accents. Berms shall not exceed 18 inches in height at top of slope.
- A 2' to 4' deep landscaped *setback* shall be provided between the *sidewalk area* and any *vertical element*.
 - Landscaping shall comprise a minimum of 80% of the landscape *setback* not including the circulation space that directly connects the park or plaza to the *sidewalk area* and seatwalls meant to face the *sidewalk area*.
- ❖ Utilities, cabinets, meters, and other similar elements shall be accommodated away from sidewalks and the frontages of the buildings adjacent to the park or plaza. These elements shall be designed integrally with the open space, not as an afterthought.
 - Screening of these elements shall follow the screening requirements in the GRD's General Standards.

Lighting

- ❖ Lighting standards shall place luminaires at a max. height of 14' to maintain a *human-scale* environment.

Environmental Resources

- ❖ Water harvesting and stormwater management facilities shall be provided within parks and plazas; they should complement the scale and urban character of parks and plazas, and be designed as integral and decorative features to the spaces.
- ❖ Water harvesting and stormwater facilities should have a dual role of managing stormwater, creating interest and providing amenities for users such as water features and sitting areas.



4. General Zoning Standards

Frontage Types

- ❖ Where a park or plaza perimeter does not abut a street, 100% of the perimeter of a park or plaza shall be lined with a Frontage Type that is allowed on the adjacent Street Frontage with the exception of *pedestrian pass-throughs*.

Notes

- ❖ **Minimum Functional Open Space:**
 - Plaza: 75%.
 - Park: 50%.
- ❖ A park or a plaza shall be open and accessible to the public from at least 6 a.m. to 10 p.m., whether it is publicly or privately owned.
- ❖ Parks and plazas shall be designed to accommodate a variety of scheduled events and programmed activities, for a wide range of users and ages.
 - Scheduled events could include small-scale musical and art performances, farmers' markets, artisan festivals and similar attractions that can draw the public into the space.
 - Programmed activities could include a tot-lot, seating around a table (either fixed or movable), and similar elements that support informal activity and interactions between users of the open space.

4. General Zoning Standards

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4. Grant District Categories

4.6 Building Height, Massing, and Articulation Standards

4.6.1 Height

Measurement

Structure Height Measurement: Per LUC Section 3.2.7.

Standards

This section provides general building height standards for the GRD. Additional standards for building heights are provided in the dimensional standards for each zoning category in Section 3 Grant Road District Categories.



Figure 4.6.2.a: Photos of example buildings with massing that is scaled to context. The first photo shows building massing appropriate to the intersection of two major mixed use commercial streets, while the second photo shows a building with varied scale and articulation of massing transitioning from a busy commercial street to the existing single-family home on the right.

Maximum Building Heights

Maximum building height limits are set in Section 3 Grant Road District Categories. Each category has a Site Maximum that is the maximum height allowed for lots with that category. Specific maximum heights are defined for frontages along Major Streets, non-Major Streets, property boundaries with non-Residential zones and property boundaries with Residential zones.

Height Transitions within the GRD

Where a GRD Category abuts another GRD Category with a lower maximum height limit, the following height transition requirement applies: Within 20 feet of the GRD category with the lower height limit, the height limits of adjacent GRD Categories are the lesser of 11 feet higher than that of the GRD Category with the lower height limit or the GRD height limit for the Category.

Minimum Upper Floor Stepbacks

Each GRD Category contains a set of minimum upper floor stepbacks, which dictate the way each maximum height transitions from a lower height maximum at the edge of the buildable area to the Site Maximum Building Height. See Maximum Building Heights table in Section 3 Grant Road District Categories.

Ground Floor Height

Each GRD Category contains a set of minimum clear ground floor ceiling heights for non-Residential uses fronting onto streets or open spaces.

4.6.2 Massing and Articulation

Standards

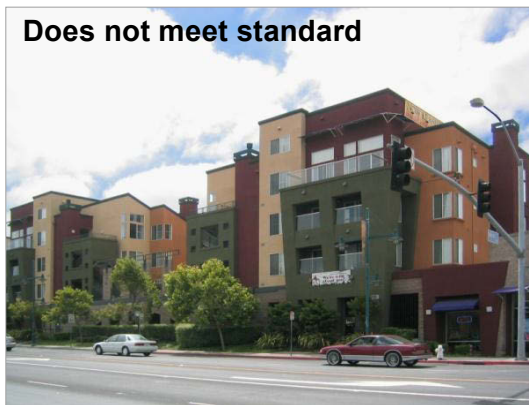
Additional *massing* and *articulation* standards can be found in Section 4.5.7 Frontage Types for each of the defined frontage types including standards for blank walls, entry distances, and glazing.

4. General Zoning Standards



Meets standard

Figure 4.6.2.b: Photo of example building that meets the standard for massing with a logical rhythm and order, where the massing articulation relates to the rooms within the building, and frames both ends of the building.



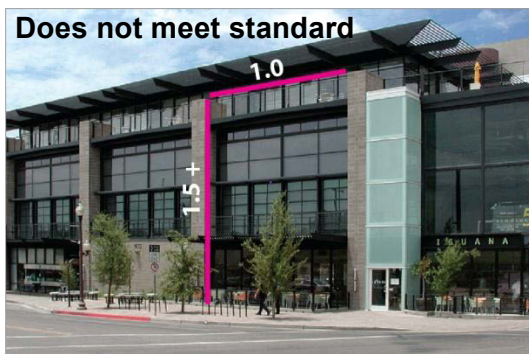
Does not meet standard

Figure 4.6.2.c: Photo of example building that does not meet the standards for massing with a logical rhythm and order, where the building massing is arbitrary and chaotic.



Meets standard

4.6.2.d: Photo of example buildings that meet the standard for massing with a logical rhythm and order (also see Figure 4.6.2.i for an example of an undesirable facade).



Does not meet standard



Meets standard

Figure 4.6.2.e and 4.6.2.f: The building on the left satisfies the GRD standard of 1:1.5 horizontal to vertical building massing ratio while the building on the right displays a horizontal massing that does not meet the standard.

4. Grant District Categories



Figure 4.6.2.g: Photo example of buildings that satisfy the massing and articulation standard for a base and top that are human-scaled.

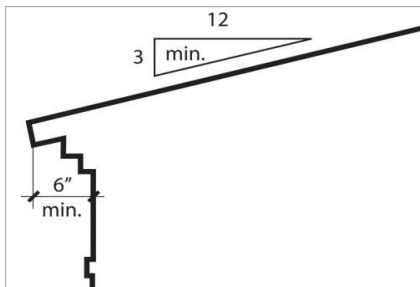


Figure 4.6.2.h: Diagram of minimum roof pitch and articulated overhang

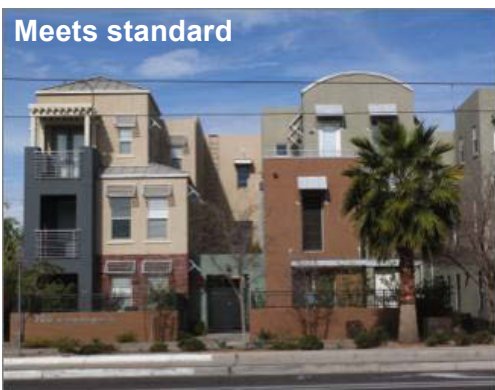


Figure 4.6.2.i and 4.6.2.j: Roofs should be simple in form. The building on the left does not meet this standard as the roof massing is overly complex while the building on the right has a more ordered variety in roof form.

Massing

- ❖ *Building massing* shall be scaled to the context when there is an established character, see Figure 4.6.2.a.
- ❖ *Massing* shall follow a logical rhythm and order, and reflect the interior spaces of buildings through the use of such elements as bays, recesses, stepbacks, tower elements, loggias, roof composition, and overhangs, see Figure 4.6.2.b through 4.6.2.d.
- Massing* elements shall provide vertical *articulation* at a ratio of 1 in width to 1.5 in height to avoid an overly horizontal building form, see Figures 4.6.2.e and 4.6.2.f.
- ❖ Ground floor building recesses greater than 5 feet in depth shall be reserved for active uses such as dining.
- ❖ Blank wall standards: See individual frontage types in the Section 4.5.7 Frontage Types.
- ❖ Buildings shall be defined by a base and a top that are *human-scaled* in form, and particularly in height and *articulation*, see further discussion under the *Articulation* subheading of this section, see Figure 4.6.2.g.
- ❖ Building facades above a specific number of floors may not continuously extend along a frontage for more than a specific length according to the following table:

GRD Zoning Category	Above this height	Maximum continuous frontage length:
Center Marketplace	2 stories	100 feet
Center Hub	2 stories	100 feet
Center Neighborhood	2 stories	100 feet
Center General	2 stories	100 feet
Segment West	2 stories	100 feet
Segment Central	Ground	100 feet
Segment East	2 stories	100 feet

Roofs

- ❖ Sloped roofs shall be composed of a 3:12 pitch or higher with a well-articulated overhanging eave with a minimum depth of 6 inches, see Figure 4.6.2.h.
- ❖ Roof forms shall create simple profiles with no unnecessary changes in plane. Roof *articulation* shall reflect a building's floor plan and avoid being overly articulated forms, see Figures 4.6.2.i and 4.6.2.j.

4. General Zoning Standards



Figure 4.6.2.k through 4.6.2.m: Photo examples of building articulation. Figure 4.6.2.k above does not meet the articulation standards because it relies on color changes without changes in mass or materials while the building in Figure 4.6.2.l does not have enough articulation of the upper facade. Figure 4.6.2.m is an example of a building that satisfies the articulation standards with a combination of massing, materials, and color.

Articulation

- ❖ *Building articulation* shall follow a logical rhythm and order and reflect interior spaces. Use fenestration, material and color changes, expression lines, awnings, arbors, balconies, rooflines and window and door recesses to create shadowlines on *building facades*.
- ❖ A well-defined base may be composed of thicker walls, richly textured materials (including, but not limited to masonry or tile), special materials (including, but not limited to ceramic tile, granite, marble, and/or darker colored materials and/or panels) and other elements that serve to “ground” the building.
- ❖ A recognizable top may utilize cornice treatments, roof overhangs, eaves and brackets, stepped parapets, richly textured materials (including, but not limited to tile or masonry treatments), a shading element, and/or differently colored materials or other method of visually distinguishing the top from the base and body of the building.
- ❖ Color change as a sole means of *articulation*, without a change in material or building plane, is prohibited. See Figures 4.6.2.k through 4.6.2.m.
- ❖ Side facades that are visible from the *public realm* shall be articulated in a similar manner as the main façade to avoid false fronts. See Figure 4.6.2.n and 4.6.2.o.
- ❖ Building projections such as arbors, overhangs, awnings and canopies are encouraged along facades at the ground floor to provide shade, human-scale, and help to define the building’s “base.”
- ❖ Awnings and canopies shall be proportional to the façade on which they are placed and not obscure architectural elements and details.
- ❖ Awnings shall be no wider than a single storefront, architectural bay or window grouping.
- ❖ Awning materials shall be consistent with the architectural style of the building. Use of glass, metal, wood, or canvas materials is encouraged. Vinyl, plastic and internally illuminated awnings and *awning signs* are prohibited.



Figure 4.6.2.n and 4.6.2.o: Photo examples of building facades that are visible from the public realm, but that do not front onto a street. The building on the left provides a similar level of articulation to the street fronting facade while the building on the right does satisfy this standard.

4. Grant District Categories

Materials

- ❖ Buildings shall incorporate *human-scaled* materials that are visually complex and tactile, see Figure 4.6.2.p.
- ❖ Changes in materials shall not occur at external corners, but at interior corners or at a change in horizontal plane, see Figure 4.6.2.q.
- ❖ Materials, material combinations, and their composition shall reflect the regional and nearby contextual architectural character of a building.
- ❖ Materials such as scored plywood (i.e., T-111) siding, vinyl siding, thin brick paneling and ‘Dryvit’ type systems are prohibited.

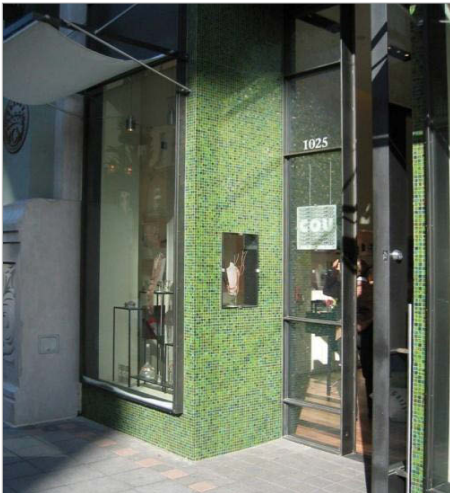


Figure 4.6.2.p: Example of a building design with human-scaled, complex and tactile building materials.

Entrances

- ❖ Main entrances shall be *human-scaled* in composition and *articulation*, see Figure 4.6.2.r.
- ❖ Buildings shall have a clear and distinct main entrance that is hierarchically defined through scale, greater *transparency* and/or a higher level of *articulation* or similar architectural treatment to distinguish main entrances from subordinate entrances, see Figure 4.6.2.s.
- ❖ Porches and stoops shall be designed as an integral architectural feature of the main structure, see also the requirements for the Porch frontage type.
- ❖ Garage doors shall be recessed a minimum of 18 inches, with the exception of service garage doors, which shall follow the standard defined by the Service Garage frontage type in Section 4.5.7 Frontage Types.
- ❖ Building *setback* and *build-to line* requirements exclude recessed entries.

Windows

- ❖ Window designs shall maximize interior daylighting while reducing glare. Mirrored or smoked glass shall be avoided and their use limited to *building accents*. Use passive shading devices, special low E glass technologies, deep overhangs or external sun shading devices and landscaping to maintain visibility between the exterior and interior of the building.

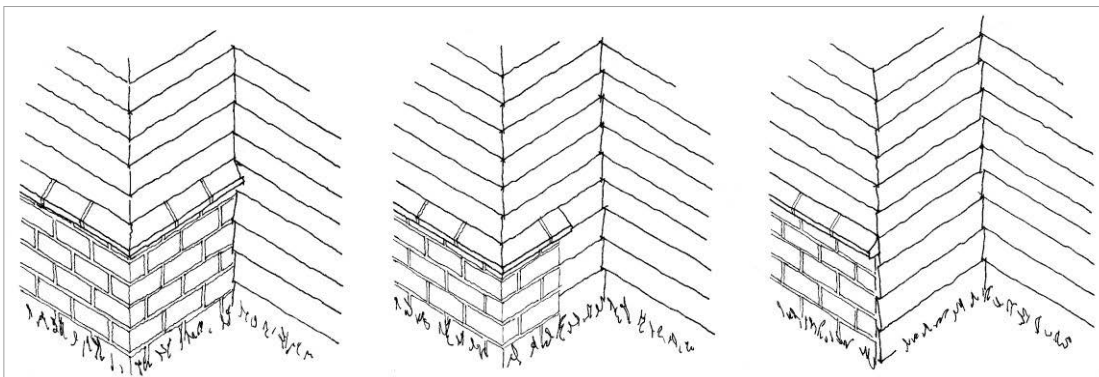


Figure 4.6.2.q: Diagram of material changes in relation to external corners. The illustration on the left meets this standard, because the material change occurs at an internal corner. The other two conditions do not satisfy the standard. The center illustration has the material change where there is no change in horizontal plane and the illustration on the right has the material change at an external corner.

4. General Zoning Standards

- ❖ *Clerestory windows* as the sole fenestration along the main façade shall be limited to the blank wall lengths as defined for each frontage type in Section 4.5.7 Frontage Types for the purposes of calculating *articulation* and *transparency*.
- ❖ *Transom windows* are allowed on any frontage and should not be confused with *clerestory windows*.
- ❖ The combined area of temporary and permanent window signs shall not obscure window area by more than 25%.
- ❖ The obscuring of window area by interior shelving and display not meant for outdoor interest is prohibited.
- ❖ Shades, curtains, blinds, and other permanent or non-permanent light control elements shall not reduce the amount of *transparency* beyond the blank wall requirements.

Development Transition

- *Building massing* transitions in relation to existing adjacent residential zones, including those across an alley, shall follow standards as defined within each of the Grant Road District Categories.
- Buffers and/or screening consistent with the purpose of this section shall be provided between a developing site and affected residential properties. See LUC Section 3.7.2.4.B Table 3.7.2-I for requirements. between uses, with the following exceptions and clarifications:
 - Screening requirements of LUC Table 3.7.2-I between development and streets do not apply in the GRD, see requirements in Section 4.7.7 Screening.
 - LUC Section 3.7.2.4.B.2 does not apply within the GRD.
 - Screens shall take on the character of the residential use where new development abuts historic designated buildings or neighborhoods.



Figure 4.6.2.r: Photo example of a main entrance that has composition and articulation that make it human-scaled.



Figure 4.6.2.s: Photo example of a clearly defined building entrance. This lobby entry is distinguished from the individual unit entry to the left by its large-scale awning and the entry plaza.



4. Grant District Categories

4.6.3 Privacy Considerations when Adjacent to Existing Residential Zoning

4.6.3.1 Requirement

Applicants proposing projects must design the projects to limit visual and noise intrusion into adjacent properties. While all intrusions cannot be prevented, applicants are required to provide a reasonable level of privacy mitigation, which can be achieved by complying with the requirements of this section. This section is largely based on the Jefferson Park Neighborhood Design Manual's Chapter 5: Privacy Mitigation.

4.6.3.2 Applicability

- Privacy mitigation is required when:
 - 1) the following types of development are proposed adjacent to existing single-story residences:
 - Construction of a multistory building;
 - Addition of a story to an existing building; or,
 - Additions to existing second floor or higher stories;and:
 - 2) the façade of the building in question is within 10 feet of the minimum *perimeter yard* or stepback for the façade (see Category Standards), up to a maximum distance of 40 feet from the property line shared with the adjacent property. Where an alley runs between properties in question, half of the alley's width may be counted toward the maximum distance of 40 feet.
- ❖ Privacy Mitigation is encouraged, but not required of proposed single-story projects.
- ❖ Where a two-story building is proposed adjacent to existing two story residences, Privacy Mitigation regarding location and screening of balconies is encouraged, but not required.

4.6.3.3 How to Use this Section

Privacy mitigation is divided into two categories: Windows and Balconies. Each category has Requirements and Strategies. Applicable projects must comply with the Requirements of each applicable privacy mitigation section. The Strategies are recommended ways of accomplishing the requirement.

Applicants may propose strategies other than those provided if the objective is met. The Planning and Development Services Director will determine if the alternate strategy meets the requirement's intent.

- ❖ The Design Professional may require a specific strategy or allow optional strategies when the applicant's design proposal does not comply with a reasonable level of privacy mitigation.

4. General Zoning Standards

4.6.3.4 Mitigation of Privacy Intrusions from Windows

Windows can cause intrusion into the privacy of neighboring properties. This can occur in two ways: when windows of neighboring properties directly face each other, and when windows of one building face into the private yard of a neighboring property. Chances of the latter occurring are much greater and more difficult to mitigate in the case of upper floor windows. Types of privacy intrusion from windows and various ways to mitigate intrusions are outlined below.

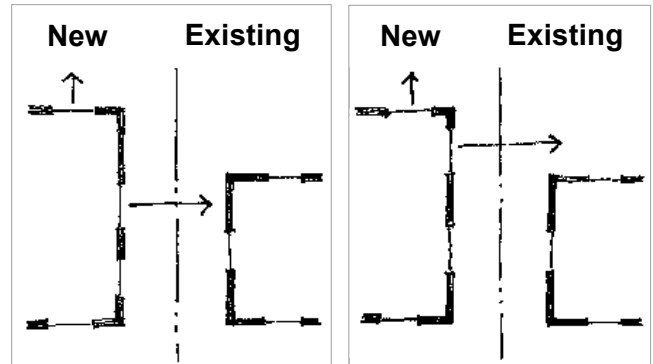
- ❖ **Requirement:** Upper-floor windows must be designed, located, and/or treated to minimize direct views from upper story windows of new multistory development into windows and private outdoor living spaces of neighboring buildings. When the City’s adopted Building Code requires an accessible window on an upper floor for safety reasons and the window would directly overlook a private yard or provide a direct view into an adjacent property’s window, the window must be translucent up to a height of at least five feet above the interior finish floor of the new building.

Strategy #1: Orient windows away from neighbors’ private yards and windows, see Figure 4.6.3.4.a.

Strategy #2: When a new window must face a neighbor’s yard the window should have the sill at least five feet above the floor of the new building or a translucent window should be used up to at least five feet above the finish floor.

Strategy #3: Where an existing single story residence exists, adjacent rear-facing, upper floor windows should be recessed into the building to avoid side views into neighbors’ private yards, see Figure 4.6.3.4.b.

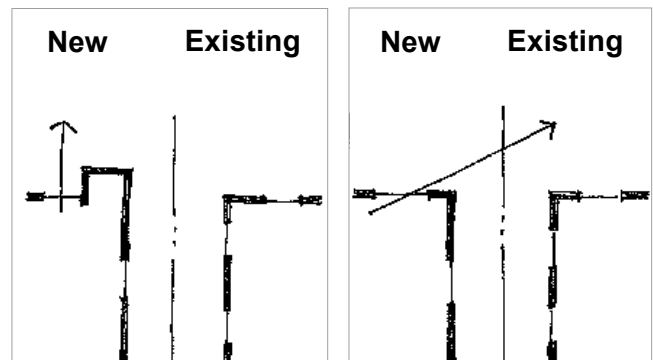
Strategy #4: Set the upper floors back from the first story to provide greater distance from upper floor windows into neighbors’ private yards, see Figure 4.6.3.4.c.



Preferred: Window facing away from neighbor's yard

Avoid: Windows facing neighbor's yard

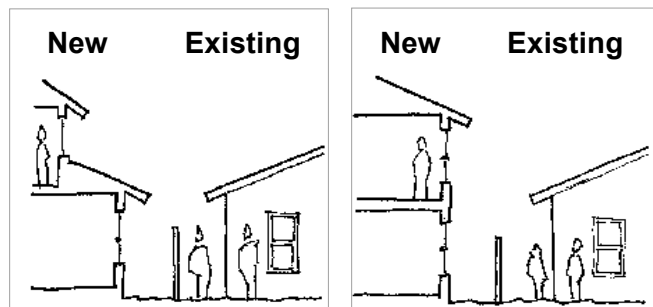
Figure 4.6.3.4.a: Illustration of a strategy for orientation of windows abutting an existing property, and window orientations to avoid.



Preferred: Upper floor windows recessed into buildings

Avoid: Upper floor windows at face of building with side view into a neighbor's yard

Figure 4.6.3.4.b: Illustration of a strategy for orientation of upper floor windows abutting an existing property, and window orientations to avoid.

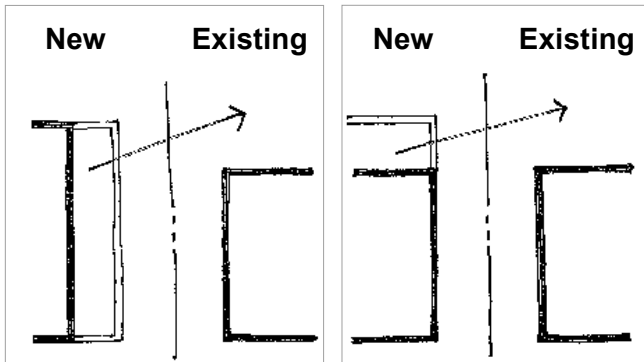


Preferred: Upper floor set back from 1st story

Avoid: Upper floor even with 1st story

Figure 4.6.3.4.c: Illustration for strategy for upper floor windows oriented to a neighboring property, and orientations to avoid.

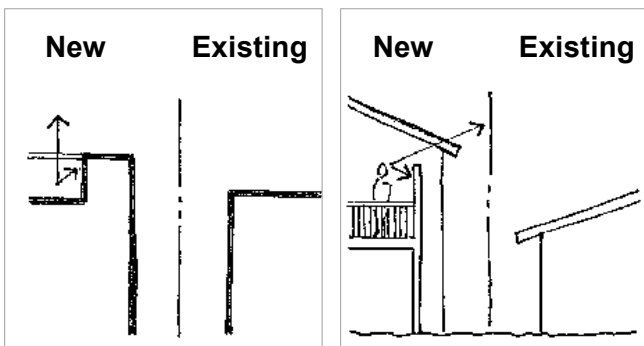
4. Grant District Categories



Avoid: Side or rear balcony overlooking a neighbor's yard.

Avoid: Corner balcony overlooking a neighbor's yard.

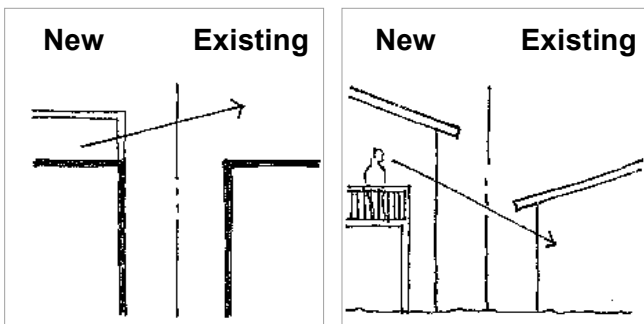
Figure 4.6.3.5.a: Illustration of balcony orientations to avoid.



Preferred: Balcony recessed into the house to prevent view into neighbor's yard.

Preferred: Balcony with 6' high opaque screen to prevent view into neighbor's yard.

Figure 4.6.3.5.b: Illustration of strategies for maintaining privacy between new balconies and adjacent existing buildings and yard.



Avoid: Open balcony with view into neighbor's yard.

Figure 4.6.3.5.c: Illustration of conditions that do not provide privacy between new balconies and adjacent existing buildings and yards.

4.6.3.5 Mitigation of Privacy Intrusions from Balconies

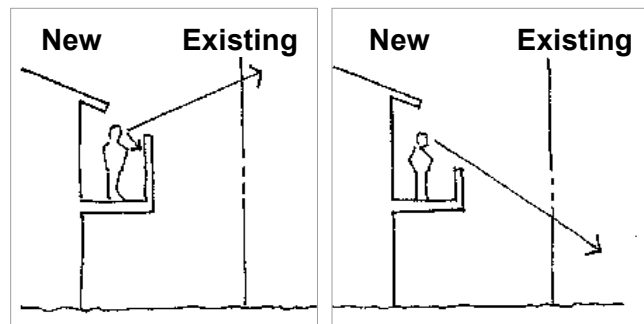
Poorly designed or improperly placed balconies can cause noise and visual intrusions into neighboring properties. Types of privacy intrusion from balconies and various ways to mitigate these intrusions are outlined below.

- ❖ **Requirement:** Balconies must be designed, located and/or screened to mitigate the visual intrusion into neighbor's windows and private outdoor living space.

Strategy #1: Locate balconies so that they do not provide views into a neighbor's private yard, see Figure 4.6.3.5.a for balcony orientations to avoid.

Strategy #2: A balcony can be recessed into the house or provide a six-foot high opaque or translucent screen at the side of the balcony to prevent views into the neighbor's private yard, see Figures 4.6.3.5.b and 4.6.3.5.c.

Strategy #3: Where a rear facing balcony overlooks a neighbor's rear yard and is less than 25 feet from the neighbor's rear property line, a six-foot high opaque screen should be provided on the balcony to prevent views into the rear neighbor's private yard, see Figure 4.6.3.5.d.



Preferred: Balcony with 6' high screen preventing view into neighbor's rear yard.

Avoid: Open balcony with view into neighbor's rear yard.

Figure 4.6.3.5.d: Illustration of strategy to maintain privacy between a new upper story balcony and an existing adjacent rear yard, and conditions to avoid.

4. General Zoning Standards

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4. Grant District Categories

4.7 Site Design Standards

4.7.1 Parking

General Parking

Purpose

This section establishes minimum and maximum requirements for motor vehicle and bicycle parking to ensure that such facilities are consistent with the objectives of the General Plan, and the Area and Neighborhood Plans that include the Grant Road District, and with the Vision for Grant Road Centers and Segments. These regulations are intended to promote public safety and environmental quality. Specifically, these regulations are intended to:

- Ensure sufficient off-street motor vehicle and bicycle parking facilities by establishing minimum parking requirements for land uses;
- Reduce excessive off-street parking by providing parking maximums, encouraging the shared use of *vehicular use areas* and the use of public transit;
- Promote pedestrian safety, comfort and convenience by providing pedestrian networks within parking areas, and to establish a network that could become local streets as revitalization continues to occur over time;
- Encourage safe, convenient, and efficient design of motor vehicle and bicycle parking spaces, circulation, and access areas;
- Provide shade to increase comfort and reduce urban heat island effect;
- Contribute to the aesthetic improvement of the Grant Road planning area;
- Improve the watershed by managing rainwater runoff; and,
- Improve air quality by requiring paving of *vehicular use areas*.

Surface Parking Lots in the Grant Road District will accommodate the need for motor vehicle and bicycle parking for nearby uses while also maintaining attractive *street frontages* that are safe and comfortable for pedestrians. Parking lots will also provide a safe, comfortable, and convenient circulation network for pedestrians and bicyclists, and landscape that provides shade and counterbalances the negative watershed and climate impacts of large impermeable surfaces.

Parking Structures in the Grant Road District need to be designed to have the visual appearance similar to the commercial and residential buildings that they serve, rather than disrupting the character of the built environment as parking structures often do. See Figure 4.7.1.a.

General Provisions

- **Bicycle Parking Requirement Calculations:** Per LUC Sections 3.3.3.6 and 3.3.8.2B.
- **Parking for Physically Disabled:** Per adopted Building Code of the City of Tucson, see LUC Section 3.3.3.2.
- **Location Requirements:** Per LUC Section 3.3.6.2 except amend distance from use being parked (in 3.3.6.2.A.1) to 1/4 mile.
- **Fractional Amounts:** Per LUC Section 3.3.3.7.
- Areas that may not be used for Required Parking, Per LUC Section 3.3.6.3.
- **Use Changes and Expansions Resulting in Changes in Parking Requirements:** Property owners and developers should consider the range of land uses that will use the space over the life of the development and the ability to share parking among uses on the site and with other nearby properties, and then document and provide the needed parking capacity and flexibility for the likely future uses of the site.

4. General Zoning Standards

Required Number of Motor Vehicle Parking Spaces

Minimum number of spaces required

- Required minimum number of motor vehicle parking spaces: Per LUC Section 3.3.4.
- Applicants may use shared parking calculations and *District Parking Programs* to reduce and manage minimum parking requirements, see Shared Parking and *District Parking* below.

Maximum amount of parking allowed

Uses may not exceed the minimum number of parking spaces required by LUC Section 3.3.4 by more than the following multipliers:

- Grocery Stores: 1.50
- Other retail above 30,000 square feet leasable area: 1.40
- Other retail and restaurants: 1.35
- Multifamily residential: 1.35
- Single-family residential: 1.50
- Office: 1.25
- Industrial: 1.25

Standards

General Standards

- **Parking Space Dimensions:** Per LUC Section 3.3.6.4, including Figure 3.3.6.4.A-I and Table 3.3.6.4.A-II.
- **Compact Spaces:** Up to 10 percent of the total number of parking spaces for a given site may have decreased dimensions as described in LUC Section 3.3.10.4, "Parking Design Modification Request." These decreased parking spaces must be marked as "compact" and must not be less than 8 feet in width.
- Lighting deflection from residential areas, Per Section 3.3.6.10 of LUC.
- Use of alleys and street for maneuvering in and out of parking spaces is allowed.
- **Tandem Parking:** Allowed for Residential Care Services with four (4) or fewer spaces provided, single-family residential development, individual multifamily units, mobile home dwellings, duplexes on individual lots, or attendant parking in commercial parking lots.
- If parking access is provided through an alley and the access would result in more than 30 peak hour trips and a residential use abuts any portion of the alley providing access to adjacent streets, the owner of the property implementing the GRD is responsible for ensuring that a six-foot high masonry wall is present between the alley and the residential use to minimize noise. If the owner of the residential property(ies) does not desire the wall, the requirement can be waived.



Figure 4.7.1.a: Photo examples of ground floor commercial with parking above, note that the parking garage in the example on the right does not meet the design standard for architectural expression that is similar to surrounding buildings, because of the large horizontal openings into the garage.



4. Grant District Categories

Surface Parking Lot Design Standards

Surfacing requirements: Per LUC Section 3.3.6.9, with the addition of paver blocks to list of allowed materials. Paver blocks are pre-cast pieces of concrete or brick that interlock to create hardscape surfaces. Pavers can create a more inviting surface than asphalt or concrete. See Figure 4.7.1.b for illustrative examples of meeting surface Parking Lot Design Standards.

Landscape¹

These standards supersede City of Tucson Development Standard 2-06.3.3.

- ❖ A canopy tree shall be provided for every 3 spaces:
 - Each tree must be planted in a planting area with a minimum horizontal dimension of 4' in all directions. An exception to this is where parking is designed for angled spaces and triangular areas are left at the ends of parking spaces, as long as the average width of the triangle is at least 4'.
 - Trees in the parking lot buffer along the sidewalk mandated by the Parking Lot Frontage Type standards or other landscaped area can count toward this requirement but the tree must be within 5' of the parking lot.
 - When planted within the *vehicular use area*, trees should be located at the edge of and between vehicle spaces, such as the common corner of four perpendicular spaces that face each other.
 - Trees should be planted "orchard style," meaning as evenly spaced as possible within the parking lot design to create shade throughout the parking lot.
- ❖ An alternative to fulfilling the requirement of 1 canopy tree for every 3 spaces is the provision of trees and/or shade structures that shade at least 50% of the *vehicular use area* in a dispersed pattern to create shade throughout, for the portion of the parking lot where shade structures are used. The amount of shade available on June 21 is calculated between the hours of 9:20 a.m. and 3:20 p.m.,

Mountain Standard Time. This alternative does not supersede the requirements mandated by the Parking Lot *Street Frontage* Type, screening requirements for site perimeters, or requirements for minimum percentage of the *vehicular use area* to be landscape.

- ❖ Landscaped areas at least 5' wide and with one canopy tree for every space length are required adjacent to pedestrian walks that are perpendicular to parking bays (parallel to parked cars) and parking spaces, and any pedestrian walk along a parking area access lane (PAAL). If the pedestrian walk runs along a PAAL, the landscaped area must be between the walk and the PAAL. See the Pedestrian, Bicycle, and Vehicle Circulation subheading of this section (for requirements for pedestrian walks).
- ❖ Where parking is designed for angled spaces and triangular areas are left at the end of parking rows, these spaces may count toward the above landscaped area requirement and the average width of the triangle must be at least 5'.
- ❖ Landscaping within the interior of parking lots should comprise a minimum of 10% of the total net parking area, exclusive of *street frontage* and interior property boundary landscape borders.
- ❖ Landscaped areas should be protected from cars with a raised curb six (6) inches high to prohibit the tires of the vehicle from encroaching onto the planter. Standard wheel barriers are acceptable but not encouraged, as they can be easily moved and could allow damage to trees or other landscaping. If a curb is used, low groundcovers, crushed rock or permeable pavers should be located within the area 18 inches from the face of curb.
- ❖ Parking spaces may be designed so that the front of a vehicle overhangs into planter areas that are within a *vehicular use area* but cannot overhang into the landscaped area between the *vehicular use area* and the sidewalk. The maximum amount of overhang is the same measurement as the parking space wheel stop location, as permitted in Development Standard 3-05.0.2.

¹This section incorporates standards from the Land Use Code and Development Standards as well as new standards specifically for the GRD

4. General Zoning Standards

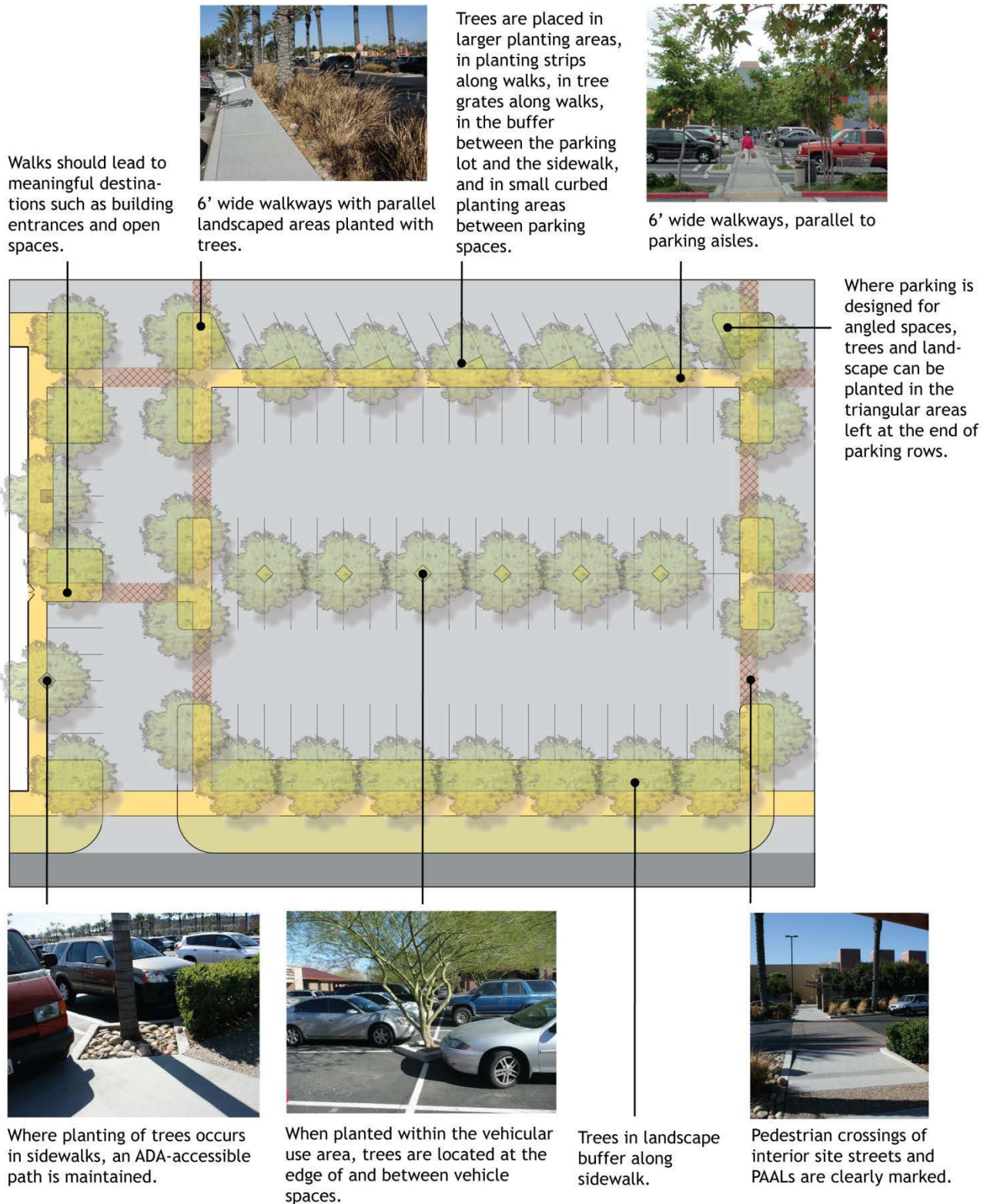


Figure 4.7.1.b: Illustrative examples of surface Parking Lot Design Standards.

4. Grant District Categories

- ❖ See Section 4.7.5 Screening for required width of interior landscape borders and screening requirements.
- ❖ See Section 4.5.5 Frontage Types for required landscape buffer between the sidewalk and the parking lot.

Rainwater Runoff Management/Harvesting

- ❖ Water harvesting techniques shall be incorporated as part of the landscape design based on the Water Harvesting Guidance Manual prepared for the City of Tucson Transportation Department Stormwater Section and Section 5.10 Environmental Resources.

Screening

- See Section 4.7.5 Screening of these General Standards.

Pedestrian, Bicycle, and Vehicle Access and Circulation

- ❖ Pedestrian circulation in and around surface parking lots should be considered as a network that is safe and comfortable to walk between building entrances, parking spaces, and the sidewalks on surrounding streets. Walks should lead to meaningful destinations such as building entrances and open spaces. Larger parking lots should be designed such that drive lanes, pedestrian circulation, landscape, and underground utilities are configured to become future streets and blocks. See Figure 4.7.1.c.

- ❖ 6' wide minimum walkways are required perpendicular to parking bays (parallel to parked cars) no further than 20 spaces apart. These walkways are required to have parallel landscaped areas planted with trees (see Landscape subheading of this section, above).
- ❖ 6' wide minimum walkways are required at the end of every row, along drive aisles.
- ❖ 6' wide minimum walkways, clear of vehicle overhangs, parallel to parking aisles (perpendicular to parked cars) are required at a minimum spacing of every four rows. Planting of trees to fulfill the one tree-per-3-spaces requirement may occur in this area, but an ADA-accessible path must be maintained.
- ❖ Pedestrian crossings of interior site streets and PAALs must be clearly marked.
- ❖ Where a parking lot abuts a building there must be a 6' wide minimum pedestrian walkway clear of vehicle overhangs. Planting of trees to fulfill the one tree-per-3-spaces requirement may occur in this area, but an ADA-accessible path must be maintained.
- ❖ Walkways required above perpendicular to parking bays and parallel to parking bays should create a pedestrian circulation network, aligning with each other as much as possible and be connected with marked crosswalks.
- ❖ The number of driveways into surface parking lots should be minimized. Driveway widths should not exceed 24 feet and should be designed to minimize interference with pedestrian traffic flows.

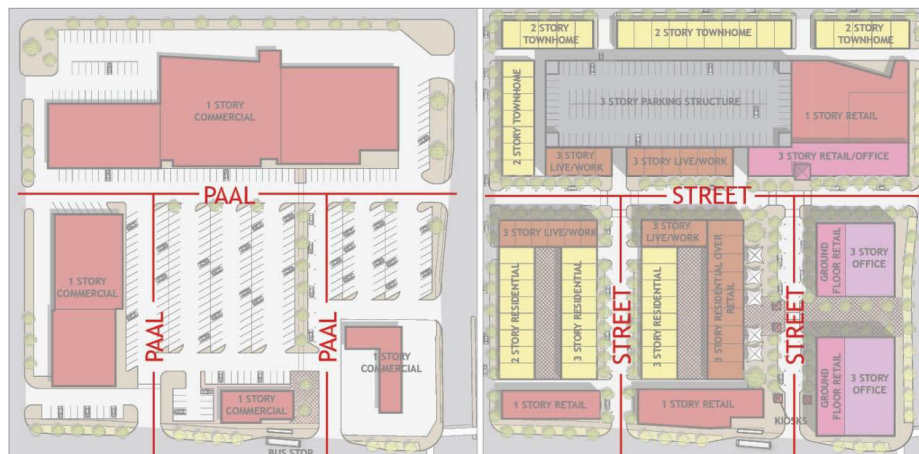


Figure 4.7.1.c.: Illustration showing the conversion of a main parking aisles and walkway connections into new streets within a shopping center redevelopment.

4. General Zoning Standards

Lighting

- ❖ See Section 4.8 Lighting for lighting requirements of parking lots.

Frontage Types

- ❖ Where a surface parking lot abuts a public street, the design standards of that frontage are governed by the Parking Lot frontage type. See Parking Lot frontage type in Section 4.5.5 Frontage Types.

Shared Parking

Purpose

Shared parking agreements may reduce parking minimum requirements for uses with different times of peak parking demand. Shared parking provides for more efficient use of parking which provides economic and environmental benefits.

A shared parking agreement must be prepared in a manner acceptable to the Director per LUC Section 3.3.5.1.D.10.

Shared Parking Provision

The number of parking spaces required for two more contiguous uses may be reduced based on an analysis of peak-hour parking demand established by means of the Urban Land Institute's shared parking methodology as described in the latest edition of the Urban Land Institute's (ULI's): "Shared Parking" manual² and provided the following standards are met.

Standards

- Individual Parking Plan (IPP): Per LUC Section 3.3.5.1 with the following amendments, additions, and exceptions.
 - 3.3.5.1.B Applicability – additional allowance for application of an IPP to more than one development through an agreement between parties.
 - Allowance for reduction in parking for multiple uses with differing peak-hour usage as determined based on the ULI shared parking methodology.
 - 3.3.5.1.C Permitted Uses and Types of Development is not applicable within the GRD as the category requirements of the GRD define use requirements for the GRD.
 - 3.3.5.1.E.2 – amended to require that all parking is provided on-site or at an off-site location per the requirements of the GRD.
 - 3.3.5.1.E.8 – amended so that outdoor lighting shall comply with the requirements of the GRD.
- Shared parking may be used to manage parking within a single property or a grouping of properties.
- If the properties sharing a single shared parking facility are separated by a major street, or if the number of properties sharing the parking is four or more, the parking facility must be managed as *district parking* and meet the standards of the GRD's *District Parking* zoning requirements.
- If shared parking area includes 50 spaces or more, Individual Parking Plan must include an ongoing parking management monitoring program that includes a periodic report submitted to the City of Tucson. The report shall include analysis of parking utilization in the shared parking area and determination of whether parking is overflowing into adjacent neighborhoods.

² The current edition of this reference document is—*Shared Parking, Second Edition*; the Urban Land Institute (ULI) and the International Council of Shopping Centers (ICSC), published by the Urban Land Institute, 2005.



4. Grant District Categories

District Parking

Purpose

District parking facilities are shared parking areas that serve multiple purposes in Centers and Segments along Grant Road:

- They help accommodate the need for motor vehicle parking along Grant Road by supplementing or supplanting parking lots for individual developments and uses with shared parking spaces.
- They increase efficiency of parking by allowing a variety of uses with different peak parking periods share parking.
- They orient patrons of businesses to the Center or Segment they are in.
- They reduce vehicle trips by allowing people to park once for multiple destinations within the Center or Segment.
- They provide flexible public space that can easily be converted into space for events or gatherings such as markets or festivals.
- They provide a sense of identity for Grant Road and its Centers and Segments.
- They can make use of remnant parcels acquired for the Grant Road Improvement Project that would otherwise be difficult to develop.
- They can provide a model for rainwater runoff management and harvesting surface parking standards and for the design standards for structure parking buildings.

Applicability

These standards are applicable only to parking lots designated as *District Parking Areas* in the Grant Road District.

District Parking Policies

- Remnants of parcels acquired for right-of-way for the Grant Road Improvement Project may be used to build *district parking* facilities.
- A property owner or a business group must reach an agreement with the city for construction, maintenance, and management of the parking facility.
- A *district parking* facility may also be management by TDOT or another city department should the City decide to implement such a program.
- Proximity to a *district parking* facility can reduce the amount of parking required for a given applicant. As part of a shared parking plan to be approved by the Director.

4. General Zoning Standards

District Parking Design Standards

District Parking Lots

Standards for Surface Parking Lots apply to *District Parking* Lots, with the following added requirements and allowances. See Figure 4.7.1.d.

- ❖ **Orientation:** Lots must include a small area at the entry to the parking area adjacent to and directly accessible from a public sidewalk that serves as a place to orient users to surrounding businesses and other uses. This area must be shaded, must have a directory sign listing nearby businesses that are participating in the *district parking* program, must have an iconic sign at the entry visible from the roadway branding the parking facility, and should have seating as appropriate for the parking area to also function as a public gathering space.
- ❖ Design must take into consideration the possibility of conversion from free to paid parking.
- ❖ Lots must be surfaced using permeable materials, such as paver blocks and permeable concrete.
- ❖ Pedestrian areas should exist at the same grade as PAALs and parking spaces and should be delineated by bollards or similar barriers and changes in paving

texture, rather than curbs. This contributes to the *district parking* lot character as a shared space between people and cars, and allows for use of the space for purposes other than parking, such as farmers markets.

- ❖ If site constraints prevent the provision of all pedestrian walks required in the Surface Parking Lot some routes may be waived at the discretion of the TDOT Director.

District Parking Structures and Underground Parking

Parking Structure and Underground Parking Design Standards apply to *District Parking* Structures, with the following added requirements and allowances:

- ❖ **Orientation:** Structures must include a small area at the entry to the parking structure adjacent to and directly accessible from a public sidewalk that serves as a place to orient people using the parking structure to its location and people emerging from the parking structure to nearby businesses and other amenities. This area must have a directory sign listing nearby businesses that are participating in the *district parking* program, an iconic sign at the entry visible from the roadway branding the parking facility, and could include a small outdoor shaded plaza with seating.

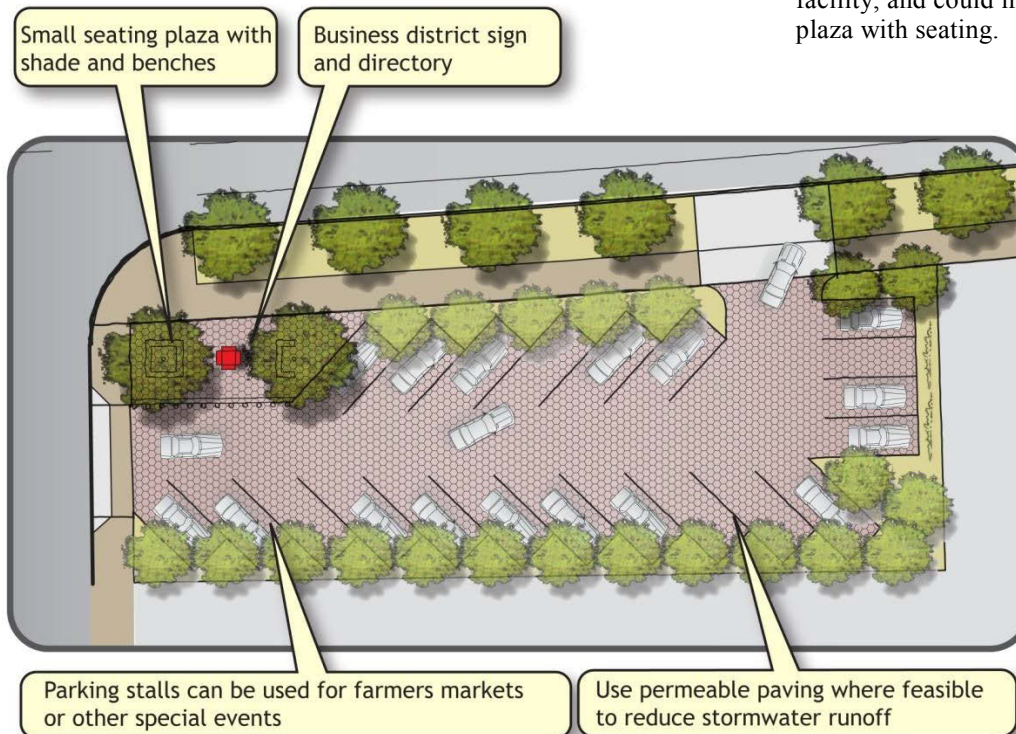


Figure 4.7.1.d: Illustrative plan drawing of a District Parking lot.



4. Grant District Categories

- ❖ Design must take into consideration the possibility of conversion from free to paid parking.
- ❖ If a *District Parking* Structure fronts onto a street or open space, that frontage must contain occupiable space at the ground level, such as the main parking entry/lobby or space designed to meet the requirements of the Public Storefront and Private Storefront frontage types.

- Adequate off-street loading facilities for new land uses and for expansion of existing land uses.
- Safe, convenient, and efficient ingress/egress for loading and unloading.
- Safe and efficient on-site vehicular circulation systems by providing separate loading areas from the vehicular traffic lanes.
- Consolidation of loading facilities on sites with multiple uses or buildings; as well as shared loading access for separate properties in order to meet the access management, pedestrian access, and other goals for the Grant Road Improvement Project and the GRD.
- The visual enhancement of the city by promoting the screening and landscaping of loading, unloading facilities, and utility areas when visible from adjacent properties or streets.

4.7.2 Bicycle Parking

Standards

Per LUC Sections 3.3.8 with the following exceptions and additions:

- The use of permeable paving for the bicycle parking area is desirable.
- Bicycle lockers are not allowed as a way to meet long-term bicycle parking requirements.
- The security requirement for long-term bicycle parking (LUC Section 3.3.9.4.B) is waived for non-residential uses when the number of long-term bicycle parking spaces is less than 10 and the long-term parking spaces and at least 25% of the short-term parking spaces are provided with weather protection per LUC Section 3.3.9.4.C.
- When structured parking is provided on-site, long-term bicycle parking should be located within the building or parking structure.

4.7.3 Off-Street Loading and Utility Areas

Purpose

This section establishes requirements for the safe receipt or distribution of materials or merchandise by vehicle to provide for:

- Needed loading and utility access for businesses and other uses while protecting safe and comfortable access for pedestrians and bicycles, as well as buffering adjacent residential neighborhoods.
- Expediting moving traffic and lessening street congestion by minimizing traffic conflicts between streets and the loading and unloading of merchandise or materials for various land use activities.

General Provisions

- Change of Approved Vehicular Use Area: Per LUC Section 3.4.3.3.
- Use changes and expansions and resulting changes in parking requirements: Property owners and developers should consider the range of land uses that will use the space over the life of the development and the ability to share loading among uses on the site and with other nearby properties, and then document and provide the needed loading capacity and flexibility for the likely future uses of the site.
- Required Number of Loading Spaces

Minimum number of spaces required

- Per LUC Section 3.4.5.

4. General Zoning Standards

Shared loading calculations

- Shared loading agreements may reduce parking minimum requirements. A shared loading agreement must be prepared in a manner acceptable to the Director per LUC Section 3.3.5.1.D.10.

Loading Design Standards

General Standards

- ❖ Per Development Standard 3-05.0, *vehicular use area* design criteria.
- ❖ **Dimensional Requirements:** Per LUC Section 3.4.4.7.
- ❖ Loading features such as bumper guards, doors, and elements for the protection of walls should be considered as part of the design of the building, not as an afterthought.
- ❖ Diagonal loading spaces and loading docks are allowed on constrained sites where standard spaces will not fit.
- ❖ **Striping of Loading Spaces:** – Per LUC Section 3.4.4.6.

Location Requirements

- ❖ The loading area does not need to be on the same property as the use, per LUC Section 3.4.4.1.A.
- ❖ Locate loading facilities within an enclosed building wherever possible, Figure 4.7.3.a.
- ❖ Where loading facilities cannot be fully accommodated within an enclosed building, loading should occur at back of building or at side and must comply with Frontage Types.
- ❖ Consolidate loading facilities on a given site to the extent feasible.
- ❖ Share loading access across adjacent sites where feasible.

Where loading must occur next to parking areas:

- Separate outside loading areas from surface parking areas (See screening requirements in this section). Figure 4.7.3.b.
- Locate loading next to employee parking (if present) rather than customer or visitor parking; employee parking areas tend to be less active than customer or visitor parking areas.



Figure 4.7.3.a: Photo example of loading facilities within the building envelope.

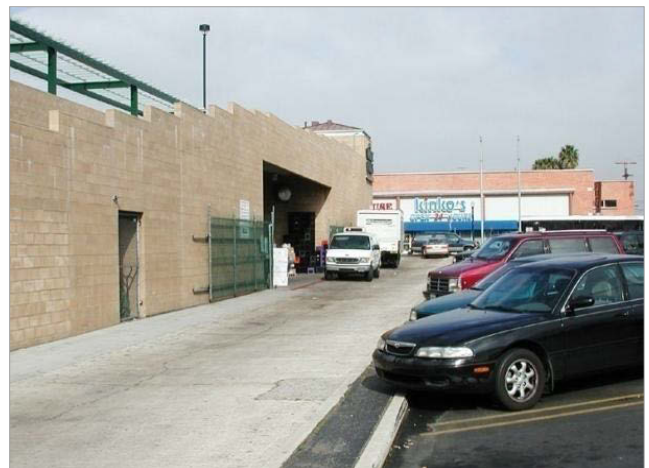


Figure 4.7.3.b: Photo example of separation between loading and parking facilities.

4. Grant District Categories

Access and Circulation Standards

- Per LUC Section 3.4.4.2 but supersede residential zone provisions related to use of loading maneuvering and 3.4.4.2 A and B with the following:
 - A street may be used for maneuvering of a delivery vehicle into, or out of, a loading space, provided the street is not shown on the Major Streets and Routes (MS&R) Plan.
 - An alley may be used for maneuvering of a delivery vehicle into, and out of, a loading space, if the alley is at least twenty (20) feet wide and is paved for its entire block length. If a residential use abuts the alley, the owner of the property where the loading is occurring is responsible for ensuring that a concrete block wall is present between the alley and the residential use to minimize noise. If the owner of the residential property does not desire the wall, the requirement can be waived.
 - Direct through-access to entry and exit drives should be provided for trucks to minimize or eliminate turning movements within a parking lot, which create impediments to pedestrian access and safety, as well as that for bicycles and other vehicles, Figure 4.7.3.c.
 - Where trucks and autos must share access lanes and driveways, lanes and driveways should be designed to the minimum width and turning movements for trucks.
 - Clear right-of-way and parking restrictions signage should be provided where truck, auto, bicycle, and pedestrian conflicts may occur within a parking lot or along the curb of a public street.



Figure 4.7.3.c: Photo example of one-way through loading. This internal loading area exits on the other side of the block.

4. General Zoning Standards

Screening Requirements

- ❖ Loading and service areas that could be visible from adjacent streets, open spaces, or adjacent residential uses must be screened from view by an enclosure. Enclosures should be designed as an integral part of the building and should be made of finished materials compatible with the design of the primary building.
- ❖ Loading docks and drop-off areas, and their associated maneuvering areas should be physically separated from parking areas via curbs, bollards, low or high walls, raised planters, landscaping, distance and/or elevation changes in order to break up the perceived amount of paving.
- For allowed screening materials, see the Section 4.7.5 Screening.
- See Section 4.7.5 Screening for required width of interior landscape borders and screening requirements for interior property lines.

Lighting standards

- **Standards for Lighting of Loading Area:** Per City of Tucson/Pima County Outdoor Lighting Code with amendments and additions as defined in Section 4.8 Lighting.

Surfacing standards

- **Standards for Surfacing of Loading Area:** Per LUC Section 3.4.4.5.

Performance standards for off-street loading

- Off-street loading located on lots adjacent to a residential use or a residentially zoned property shall restrict delivery and loading operations to the hours between 7:00 a.m. to 10:00 p.m.

Utility Area Design Standards

- ❖ Building elements, such as garbage receptacles, utility meters, and mechanical equipment should be accommodated within the building envelope to the extent possible. If such elements must be outside of the building envelope, they should be screened from the view of pedestrians. Such screening should occur in ways harmonious with the overall building design. See Figure 4.7.3.d.
- **Screening Materials:** see Section 4.7.5. Screening.

Solid Waste Facilities

- Joint-use solid waste disposal facilities are encouraged.
- On-site refuse collection container requirements governing access, type, and location may be modified if the Department of Environmental Services, Tucson Fire Department and Department of Transportation determine that no public health or traffic safety issue is created. Proposed Solid Waste and Recycling Plans shall be reviewed during the preliminary application process.



Figure 4.7.3.d: Photo example of an exterior utility screen designed to complement the architectural design of the building.



4. Grant District Categories

4.7.4 Landscape

Purpose

The majority of landscape standards for the GRD are found in other GRD sections:

- See Section 4.5.5 Frontage Types for landscape requirements for *street frontage*.
- See Section 4.7.1 Parking for landscape requirements for parking areas.
- See Section 4.7.3 Off-Street Loading and Utility Areas for landscape requirements for off-street loading and utility areas.
- See Section 4.7.5 Screening for landscape requirements for screening interior property boundaries.
- **Xeriscaping and Buffering:** Per LUC Section 3.7.1.1.

Standards

- ❖ Plazas, parks, yards and other open spaces may be delineated by an entry gateway. Entry gateways shall be a maximum of 10’ in height. Entry gateways may include features such as sunshades and arbors.
- ❖ **Street Landscape Borders:** City of Tucson LUC Section 3.7.2.4 and Development Standard 2-06.3.4 is superseded by the standards defined within Section 4.5.7 Frontage Types for each frontage type.
- ❖ **Development Standards:** subject to 2-06.0 and 2-16.0 except where noted.
- ❖ **Use of Drought Tolerant Vegetation:** Per LUC Section 3.7.2.2.
- ❖ **Vehicular Use Areas:** see Section 5.7.1. Parking; City of Tucson Development Standard 2-06.3.4 (Landscape Borders) and LUC 3.7.2.4.A is superseded by the GRD.
- ❖ **Use of Turf:** Per LUC Section 3.7.2.5.

- ❖ **Plant Size, Location and Spacing:** Per LUC Section 3.7.2.6.
- ❖ **Plant Cover/Dust Control:** Per LUC Section 3.7.2.7.
- ❖ **Design for Safety:** Safety factors must be incorporated per LUC Section 3.7.2.8.
- ❖ **Use of the Public Right of Way:** Non-required landscaping may be placed in the public right-of-way if requirements are met per LUC Section 3.7.2.9 and property owner establishes a maintenance agreement with the City Engineer.
- ❖ **Native Plant Preservation:** Per Division 8 of the LUC.

4.7.5 Screening

General Purpose Screening

Purpose

The general purpose of screening is to provide visual barriers, noise reduction, and to provide privacy. (Ord. No. 9967, §3, 7/1/04). In the Grant Road District, screening also fulfills the following specific roles:

- Maintaining a comfortable walking environment along Grant Road and its cross streets.
- Providing an adequate transition between more intensive uses on properties along Grant Road and residential areas behind.

Standards

- Development Standard 2-06.3.7.B (Screen Location Along *Street Frontages*) is superseded by the GRD.
- ❖ See Section 4.5.5. Frontage Types for screening requirements along *street frontages*.
- **Height Measurement:** Per LUC Section 3.7.3.3.
- **Sight Visibility Triangle:** Per LUC Section 3.7.3.4.

- **Phased Development:** Per LUC Section 3.7.3.5.
- **Screening Materials:** Per LUC Section 3.7.3.7, but amend to add “green screen” fences (non-chain link) as an allowed screening material, and exclude earth berms and chainlink as allowed screening materials.

Screening at Interior Property Lines

Purpose

Interior landscape borders are required as a buffer and visual transition along the common property lines between adjacent land uses. Standards within this section supersede those contained in LUC Section 3.7.2.4 B and Table 3.7.2-I. Standards defined in Section 2-06.3.7 of the Development Standards apply with the exceptions, additions, and amendments made herein.

Standards

- Interior landscape borders are required along all interior property lines as defined below:
 - Landscape buffer width: minimum of 6’ except:
 - When adjacent to existing residential zones in which case the minimum buffer width is 8’.
 - Screen height: minimum of 6’ with the following exceptions where the minimum screen height shall be 8’:
 - When adjacent to existing residential zones;
 - In the Segment West when adjacent to a GRD Center Zoning Category; and,
 - When a loading area is within 20’ of an interior property line.
- Exceptions to these standards are allowed for adjacent sites with common driveways, shared motor vehicle parking, or vehicular access easements.
 - In general, screens may consist of a wall or fence with the following exceptions where the screen shall be a wall up to its minimum screen height:
 - When adjacent to existing residential zones;
 - In the Segment West when adjacent to a GRD Center Zoning Category; and,
 - When a loading area is within 20’ of an interior property line.
 - Decorative, structural elements, such as arbors and lattice, may extend above the screen height up to a maximum of 2 additional feet above screening element.
 - Elements above screen heights shall be a minimum of 20% visually permeable.
 - ❖ Fences and walls shall be compatible with the architectural design of the less intense use.
 - ❖ Where a building within a GRD Zone sides or backs onto an adjacent R-1 or R-2 lot the requirements of Section 4.6.3 Privacy Considerations when Adjacent to Existing Residential Zoning shall be satisfied in regards to window location and design and balcony location and design.



4. Grant District Categories

4.7.6 Open Space

Public Open Space within Private Development

Purpose

The Community Character & Vitality objectives that have been defined through the Grant Road Improvement Project include a desire for public gathering places of a variety of types. Some of these are appropriate to be integrated into the mixed use and commercial developments along Grant Road, as they provide an amenity for the public and also can make the development along Grant Road more attractive to potential patrons and residents.

Standards

- Requirements for *functional public open space* are defined by Grant Road zoning category in Table 4.7.6.a.
- Courtyards, motorcourts and plazas shall have a minimum *street frontage* width as defined Section 4.5.5 Frontage Types.

- Plazas, roof gardens, roof decks, courtyards and the non-vehicular area within motorcourts in private property may be counted as part of the open space calculation so long as they are available for public use at least between the hours of 6:00am and 10:00pm.
- Vehicular space, alleys, indoor facilities and areas that do not otherwise meet the definition of *functional open space*, per the definition in Section 5 Definitions, may not be counted as part of open space calculation. For motorcourts see minimum open space standards within Section 4.5.5 Frontage Types.
 - ❖ Open space shall be integral to the design of the project and shall not encompass remnant space.
 - ❖ To the maximum extent feasible, views from open spaces shall be fronted by active spaces within buildings or public streets, building entries, or natural features to provide visual interest, and enhance safety.
 - ❖ To the extent feasible, maximize orientation of open space to southern and western exposure to encourage use throughout the year. See Section 4.10 Environmental Resources for passive solar, shading, and cooling standards.

Table 4.7.6.a: Functional Open Space Requirements for Commercial and Mixed Use Development

GRD Zoning Category	Minimum Open Space Requirement ¹
Segment Central	3% of lot area for projects greater than 10,000 s.f. of leasable area
Center Neighborhood	5% of lot area for projects greater than 20,000 s.f. of leasable area 5% of lot area for projects 10,000 to 50,000 s.f. of leasable area
Center Marketplace	10% of lot area for projects greater than 50,000 s.f. of leasable area
Center Hub	5% of lot area for projects greater than 50,000 s.f. of leasable area
Center General	3% of lot area for projects greater than 30,000 s.f. of leasable area
Segment East and West	3% of lot area for projects greater than 60,000 s.f. of leasable area

¹ Functional open space requirements for residential portion of mixed use development is defined in the Private and Common

4. General Zoning Standards

- ❖ Open spaces shall be provided with ample lighting in the evening up to 10:00 p.m. See Section 4.8 Lighting for height and design for light standards and other lighting standards.
- ❖ Open spaces shall be furnished, landscaped, and paved at a level of design and quality that encourages their use so that they do not become neglected or dangerous spaces. Elements may include planters and/or planting beds, trellises, arbors, water features, murals, decorative paving, seating, art, and decorative lighting.
- ❖ Fences and walls fronting onto open spaces may extend up to a maximum of three (3) feet in height.
- ❖ Buildings should front open spaces with active uses and primary entrances.
- ❖ Building walls and landscape walls siding onto open spaces shall be articulated and shall follow blank wall standards of the associated frontage.

Private and Common Open Space for Multiunit Residential

Purpose

Residents of single-use multiunit residential developments and those within mixed use developments have recreational needs that can be best met through both private open space that is exclusive to each unit, as well as common open space shared by the residents; common space may or may not be accessible to the general public. These standards provide for that need.

Standards

- Requirements for functional private and common open space are defined by Grant Road zoning category in Table 4.7.6.b.
- Required common open space is in addition to required private open space.

Table 4.7.6.b: Minimum Functional Open Space Requirements for Residential Development

GRD Zoning Category	Multifamily Residential		Single-Family Residential Attached or Detached
	Private Open Space	Common Open Space (% of GFA of building)	Open Space (% of lot area)
Segment Central	15% of gross s.f./unit ¹	n/a	15%
Center Neighborhood	60 s.f./unit ²	7.5% ³	10%
Center Marketplace	60 s.f./unit ²	7.5% ³	5%
Center Hub	60 s.f./unit ²	7.5% ³	7.50%
Center General	60 s.f./unit ²	10% ⁴	10%
Segment East and West	60 s.f./unit ²	10% ⁴	10%

¹ Private open space requirement may be reduced to 60 s.f. minimum for each unit if usable common open space is provided at a minimum of 12% of GFA of building.

² Up to 75% of units may have less than 60 s.f./unit of private open space if the amount of private open space reduction is added to the common open space required.

³ Common Open Space may be reduced to a minimum of %5 GFA of building where publicly accessible open space is provided on the same lot for mixed use development. A maximum of 50% of the publicly accessible open space may be counted towards this reduction.

⁴ Common Open Space may be reduced to a minimum of 7.5% GFA of building where publicly accessible open space is provided on the same lot for mixed use development. A maximum of 50% of the publicly accessible open space may be counted towards this reduction.



4. Grant District Categories

- Balconies, patios, porches, gardens, roof gardens, roof decks, rear yards, public yard frontage, private yard frontage, courtyards and the non-vehicular area within motorcourts may be counted as part of open space calculation.
- Vehicular space, alleys, and areas that do not otherwise meet the definition of *functional open space*, per the definition in Section 5 Definitions, may not be counted as part of open space calculation.
- Courtyards and Motorcourts shall have a minimum width as per defined within the Section 4.5.7 Frontage Types.
- No area shall qualify as open space unless they have a minimum clear space as follows:
 - Balconies: 6x10 feet.
 - Ground floor patios: 8x10 feet.
 - Common open space: 10x20 feet.
- ❖ Open space shall be integral to the design of the project and shall not encompass remnant space.
- ❖ Open spaces shall be fronted by active spaces within buildings to enhance safety.
- ❖ Locate common open space to maximize accessibility for all residents.
- ❖ Maximize orientation of open space to southern and western exposure to encourage use throughout the year. See Section 4.10 Environmental Resources for passive solar, shading, and cooling standards.
- ❖ Open spaces shall be furnished, landscaped and paved at a level of design and quality that encourages their use by all residents. Elements may include planters and/or planting beds, trellises, arbors, water features, murals, decorative paving, seating, art and decorative lighting.
- ❖ Design open spaces to create a variety of climate environments to facilitate activity in different seasons and weather conditions. See Section 4.10 Environmental Resources for standards.

4. General Zoning Standards

4.8 Lighting

4.8.1 Purpose

In order to achieve the community’s goals for the Grant Road District to be a vibrant, economically active, and pedestrian supportive environment, safety and business visibility are vitally important throughout the day. Good levels of lighting are important for maintaining a safe and secure pedestrian realm during the evening. Standard cobrahead and other auto-oriented lights inadequately light the pedestrian space. Pedestrians need brighter, more even, naturally-colored lighting because they look at more detail, have a smaller field of focus, move at a slower pace, and stop more frequently for longer periods of time than people moving in a vehicle. This requires shorter light standards with a higher level of design detail than typical auto-oriented lighting poles and fixtures. Well-designed pedestrian lighting can contribute to the ambience of the environment through the patterns and distribution of light as well as add to the sense of place through their appearance.

The lighting standards for the Grant Road District must balance these objectives with maintaining the City’s goals of conserving energy and resources, cost efficiency and minimizing glare and spillover lighting as well as light pollution that will interfere with the desire to maintain adequate dark skies in keeping with Tucson’s setting within a desert environment to the nearby Kitt Peak National Observatory.

4.8.2 Applicability

These standards define the characteristics of lighting and lighting fixtures that are used for lighting exterior spaces within the Grant Road District. These standards are in addition to the City of Tucson/Pima County Outdoor Lighting Code.

4.8.3 Standards

General Standards

- Within proximity to areas of potential pedestrian activity, fixtures that are closely spaced with lower light levels are preferred over fixtures spaced further apart that compensate with intense yet varied light levels.
- In areas where high levels of pedestrian activity is expected, such as at the Centers along Grant Road, Low Sodium Pressure lights (LPS) are discouraged. Metal halide light sources shall be used in pedestrian areas, streets, and parking areas for their white color of light that contributes to the comfort of users. Particular areas where higher lighting levels should be given priority are those where potential conflicts between pedestrians and vehicles occur, such as crosswalks, parking areas, as well as pedestrian spaces that will contain evening activity such as transit stops, plazas, parks and pass-throughs. In addition, routes from key uses to transit stops should also be given priority; key uses would include grocery stores, community and civic facilities, and other uses that are well patronized by transit riders.
- The preferred method for determining maximum total outdoor lighting output requirements for properties within the Centers of the Grant Road District is Option 3 Commercial and industrial light output as per Table 401.1 of the 2006 City of Tucson/Pima County Outdoor Lighting Code, with full cut-off for most lighting preferred.
- Fixture location shall maintain a regular spacing such that lighting levels remain even along a predetermined area based on an overlap of illumination at a height of seven (7) feet above finish grade.
- Fixture location shall minimize conflicts with street trees such that fixtures are not buried deep in the canopy of trees, therefore reducing their effectiveness.

4. Grant District Categories



All exterior lighting shall be designed so as to not produce glare onto pedestrian spaces and adjacent uses.

- Outdoor lighting shall conform to the following:
 - Lighting shall be shielded and directed downward.
 - Lighting within one hundred (100) feet of a residential zoning or use shall not exceed fifteen (15) feet in height.
 - All other lighting on site shall not exceed twenty-five (25) feet in height.
- Pedestrian fixture heights shall be between 12 and 20 feet high to the bottom of the luminaire. Light standards may also be combined on one post. Low, pedestrian-oriented lights can be affixed to a post and direct light onto sidewalks, while the same post may also accommodate auto-oriented lights directed at roadways.



Figure 4.8.3.a and 4.8.3.b: Photo examples of integrating exterior building lighting design to complement the architectural design of the building.

Building Lighting

- ❖ All exterior building lighting shall be an integral part of a building's architectural design. Accent lighting may be used to highlight interesting architectural features, landscaping, signs and storefront displays. See Figures 4.8.3.a and 4.8.3.b.

Bicycle Parking

- Per LUC Sections 3.3.9.2.A.5 and 3.3.9.4.E.

4. General Zoning Standards

4.9 Signs

4.9.1 Purpose

The sign standards within the Grant Road District provide flexibility in design in order to respond to a more diverse set of activities and range of character desired for the Centers and Segments along Grant Road. Signs are encouraged to be imaginative, unique and/or otherwise tasteful. They should be within the spirit and intent of the Grant Road District and objectives of the Grant Road Community Character & Vitality Plan. The sign standards take into account the range in emphasis on pedestrian-oriented character in the zoning categories within the Grant Road District.

4.9.2 Applicability

Standards contained within Chapter 3 Sign Code of the Tucson Code are applicable within the Grant Road District as modified herein. In the case of conflicting standards, the standards herein shall prevail, however in no place should the GRD standards be interpreted to be more lenient than the Sign Code.

4.9.3 Standards

General Standards

- **Bicycle Parking Signage:** LUC Section 3.3.9.4.D applies to the Grant Road District.
- **Sale of Liquor:** LUC Section 3.5.4.7.H applies to the Grant Road District.
- The General Standards provided in this section are applicable to all subcategories within the Grant Road District. Standards contained in the Chapter 3 Sign Code of the Tucson Code Article IV apply with the following exceptions or amendments:
 - The definitions in Article II. General Requirements of Chapter 3 Sign Code of the Tucson shall be amended to add glass as a material. See also Section 5 Definitions.
 - Table 4.9.3.a defines applicable Tucson Sign Code Districts as defined within the Sign Code and amended herein for each category of the Grant Road District.

Prohibited Signs

Tucson Code Chapter 3 Sign Code Section 3-53 Prohibited Signs Enumerated applies to the Grant Road District with the following amendments:

- Billboards that are freestanding and mounted or attached to buildings are prohibited; this does not include signs that are painted on building walls.
- Internally illuminated sign boxes, awnings, canopies, pole signs, projecting signs are prohibited. Internally illuminated channel letters and reverse channel letters are allowed.
- Internally illuminated menu boards are restricted to drive thru businesses and shall not exceed one per business. Light from internally illuminated menu boards shall be screened from view from streets and open spaces.

Signs by Tucson Sign Code District

A range of Sign Code Districts apply to various Centers and Segments, Frontage Types, and zoning categories of the Grant Road District. The Sign Code Districts are defined as per the *Tucson Code Chapter 3 Sign Code Article VI* with the following exceptions:

Single-family Residential District

- Per Tucson Code Chapter 3 Sign Code Section 3-72 with the following revisions:
 - Permitted signs:
 - *Awning signs:* Not allowed for single-family residential use.

O-1 Zone District

Per Tucson Code Chapter 3: Sign Code Section 3-75 with the following revisions:

- Illumination and color: signs on collector, local and new streets shall incorporate broad spectrum lighting such as external LED, halo or internally illuminated channel and reverse channel letters.



4. Grant District Categories

Table 4.9.3.a: Grant Road District Zoning Category Signage

GRD Zoning Category	Sign Code Districts						
	Single family residential	Multiple family residential	Park district	O-1 zone	Medical business	Industrial	Pedestrian business ¹
	Sec. 3-72	Sec. 3-73	Sec. 3-74	Sec. 3-75	Sec. 3-78	Sec. 3-79	Sec. 3-81
Center Hub	not applicable	applicable	applicable	applicable	applicable	not applicable	applicable
Center Marketplace	not applicable	applicable	applicable	applicable	applicable	not applicable	applicable
Center Neighborhood	not applicable	applicable	applicable	applicable	applicable	not applicable	Required
Center General	not applicable	applicable	applicable	applicable	applicable	not applicable	applicable
Segment East and West	not applicable	applicable	applicable	applicable	applicable	applicable in Segment East only	applicable
Segment Central	applicable	applicable	applicable	applicable	applicable	not applicable	Required

¹ The Pedestrian Business Sign District is required for any Public or Private Storefront frontage types, and required in any zoning category within the following specific Centers within the Grant Road District – The Fontana Center, the Center Neighborhood zoning category within the 1st Avenue Center, the Campbell Center, Country Club Center, and the Alvernon Center.

4. General Zoning Standards

4.10 Environmental Resources

4.10.1 Purpose

Environmental sustainability through efficient use of natural resources, and protection and restoration of natural resources and systems is an important goal of the Grant Road Community Character and Vitality Plan and the City of Tucson. Development in the Grant Road District can support these goals through such practices as designing for passive cooling and heating to reduce the energy consumption of development as well as providing for rainwater harvesting and stormwater management systems on site in order to protect the desert environment’s scarce water resources and mitigate flooding as feasible, see Figures 4.10.1.a through 4.10.1.c.



4.10.2 Standards

General Standards

- **Native Plant Preservation:** Per LUC Division 8 and Tucson Development Standards Section 2-15.1.0 for standards for development within the GRD.
- **Solar Considerations:** Per LUC Section 3.2.12 for standards for development within the GRD.
- **Energy Conservation:** The facilities should use little energy to run; be constructed to last; have low maintenance needs; and respect the regional, cultural, and material uniqueness of Tucson.
- **Sustainable Energy Standard:** All public facilities shall meet the requirements of the Sustainable Energy Standard to the extent reasonable. The Standard may be waived by the City Manager based on factors including, but not limited to, excessive cost or unsuitability to the project.

Passive Solar and Cooling

Site Design

- ❖ For parking lot shade provisions, see Section 4.7.1 Parking under the Landscape subheadings for Surface Parking Lot Design Standards and Off-Street Loading and Utility Areas for standards and guidance.

Figures 4.10.1.a through 4.10.1.c:
Photo examples of urban stormwater and water harvesting elements.

4. Grant District Categories



Figure 4.10.2.a and 4.10.2.b: Photo examples of shading devices that integrate with building and site design; the shading device on the left is to provide shade for pedestrians passing along the edge of a plaza and the device on the right provides shade and building articulation.

Courtyards and motorcourts shall be shaded a minimum of 50% at noon on June 21 (accounting for trees at maturity).

- ❖ Shade shall be provided for at least fifty 50% of all pedestrian pathways, pedestrian pass-throughs, outdoor plazas, parks, courtyards, gardens, outdoor bicycle parking areas and other private open space amenity, as measured at 2:00 p.m. on June 21 when the sun is 82° above the horizon (based on 32°N Latitude). Shade may be provided by trees, arcades, canopies, shade or roof structures, and/or adjacent buildings.
- ❖ Shading features shall respect the design context of the street and the architectural integrity of the building. The use of plantings in the City right-of-way is permitted to meet this standard. See Figures 4.10.2.a and 4.10.2.b.
- ❖ Deciduous trees, as proposed in the Downtown Comprehensive Street Tree Plan, are encouraged to supplement existing evergreen trees.
- ❖ Include amenities such as water features to cool outdoor spaces. Standards for water features shall follow Section 3.7.4.4 of the LUC.
- ❖ Consider prevailing wind patterns in the design of open spaces to take advantage of natural breezes to cool outdoor spaces or screen spaces from strong prevailing winds.

Building Design

- ❖ Buildings shall be designed to provide complete shade to all windows on June 21. On new construction, shading devices that require manual operation to accomplish the shading do not qualify as building design that provides shade.
- ❖ Deciduous trees, as proposed in the Downtown Comprehensive Street Tree Plan, are encouraged to supplement existing evergreen trees. Deciduous landscaping at maturity shall be integrated into the project to provide shade to eighty (80) percent of the east and west facing walls on June 21.
- ❖ On new construction, where the building does not share party walls with adjacent buildings, landscaping alone does not satisfy this criterion.

4. General Zoning Standards

Stormwater Management, Water Conservation and Irrigation, and Rainwater Harvesting

Stormwater Management

Planning and design of stormwater detention and retention facilities within the GRD shall comply with the Pima County and City of Tucson Stormwater Detention/Retention Manual, and Section 10-01.0 of the Tucson Development Standards, with the following modifications:

- ❖ **Surface Storage:** due to the urban nature of development that is expected within the GRD, it is likely that underground storage and subsurface disposal will be relied upon in the management of stormwater in most new developments.
- ❖ Depending upon the economics of new development, the use of underground storage with permeable concrete or asphalt may be appropriate for larger surface parking lots and hardscaped open spaces.
- ❖ **Aesthetic Design Guidelines:** given the urban nature of development that is expected within the GRD, the more natural or curvilinear design guidance provided in the Detention/Retention Manual may not be appropriate for most new development within the GRD. Stormwater management systems shall be configured to reflect the more urban character of the Grant Road District and integrated in ways that contribute to the aesthetics of the development as water features and amenities.
- ❖ The use of stormwater management facilities requiring security barriers should be minimized to the extent feasible in the GRD.

Water Conservation and Irrigation

- **Water Conservation and Irrigation:** Per Tucson Development Standards Section 2-06.5.0.

4. Grant District Categories

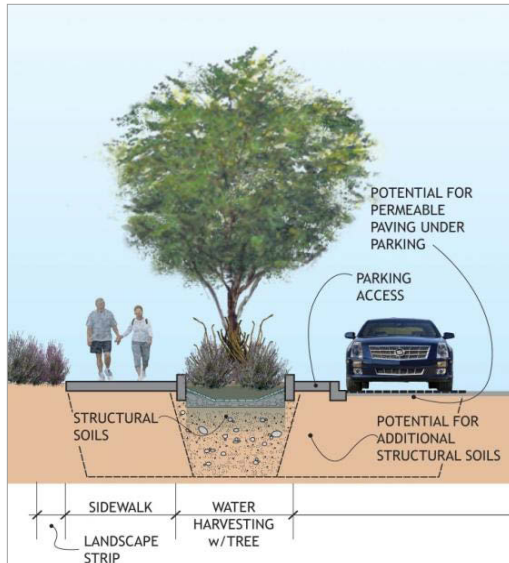


Figure 4.10.2.c: Section illustrative of tree well microbasin system.

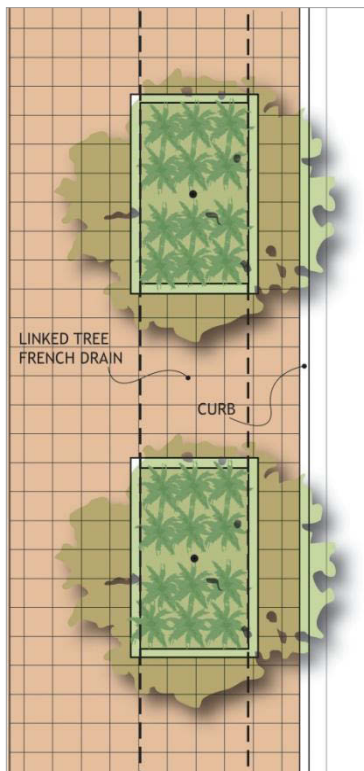


Figure 4.10.2.d: Linked tree trench french drain.

Rainwater Harvesting

Tucson water harvesting ordinance must be met per Tucson Development Standards Section 10-03.0.0 with the following amendment:

- Non-residential, mixed use, and multifamily projects within the GRD shall be subject to Ordinance No. 10597, the Commercial Rainwater Harvesting Ordinance.

Use of Water: Per LUC Section 3.7.4 with the following exception:

- Water features may be located outside of an oasis in non-residential development.

Water harvesting techniques shall be incorporated as part of the landscape, open space, parking lot, and general site design based on the Water Harvesting Guidance Manual prepared for the City of Tucson Transportation Department Stormwater Section, with the following additions and refinements:

- ❖ Given the more urban nature of the development allowed by the GRD some alternative or hybrid water harvesting techniques are appropriate within the GRD, including:
 - Tree Well Microbasins: This technique is a refinement of Microbasin Technique E from the Guidance Manual, page 5. Within a sidewalk, parking lot, plaza, or other hardscaped area, a tree well is provided with a raised curb or tree guard between the paved area and the microbasin. Structural soils, or a similar system to protect from compaction, are provided under the adjacent paving to both provide for healthy root growth and some capacity for stormwater, and to support the potential for infiltration.
 - See Figure 4.10.2.c. Permeable paving can also be provided in the area above the structural soils to more evenly distribute the storm water across the underground microbasin. In this case of open tree wells, without tree grates, additional landscape can be provided in the tree well.

4. General Zoning Standards

- **Linked Tree Trench French Drains:** This technique is an expansion of the Tree Well Microbasin technique described above. Rather than excavating for a single tree well a trench is excavated and filled with structural soil to provide for a row of trees and additional stormwater capacity. The trench can either be uncovered and with landscaping in addition to the trees, or tree grates can be used to maximize the usable area of the paved area. Permeable paving can be provided above the area of structural soil to more evenly distribute the storm water. See Figure 4.10.2.d.
- **Use of Under Drains:** Either a Tree Well Microbasin or a Linked Tree Trench French Drain can be provided with an under drain in order to avoid inundating roots for an excessive time period in cases where water flows are too high or where soil conditions do not support adequate infiltration. See Figures 4.10.2.e and 4.10.2.f.

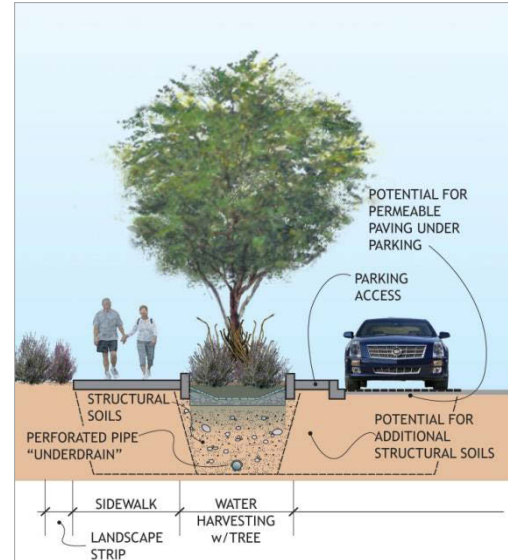


Figure 4.10.2.e: Tree well microbasin with under drain.

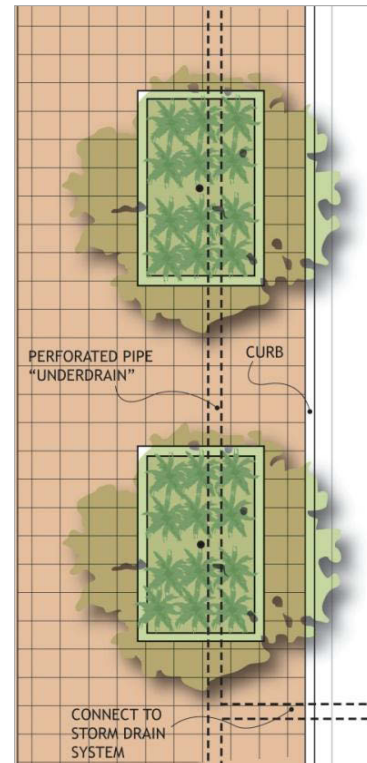


Figure 4.10.2.f: Linked tree trench french drain with under drain.



4. Grant District Categories

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5. Definitions

5. DEFINITIONS

The definitions in the LUC Division 2 Listing of Words and Terms shall apply to the GRD with the following additions and modifications.

Articulation, Architectural: The visible expression of architectural or landscape elements through form, details, and materials that define the scale of buildings and spaces to achieve a *human scale* to the building in which the elements of the building are scaled to human height, an arm’s reach, and the grasp of a hand.

Articulation, Massing: The definition of the larger form and massing of a building by structural bays, roof design, and other elements that create interest and scale to the experience of buildings from a distance or while traveling along an adjacent roadway.

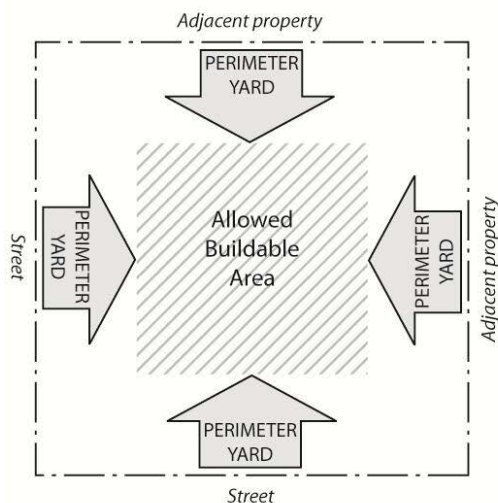
Awning sign: A sign constructed of cloth, metal, glass, and permanently affixed to a structure and intended to provide shade. This definition amends Article II. General Requirements of Chapter 3 Sign Code of the Tucson Code.

Building Façade: The external face of a building fronting onto a street or open space or that is visible from the *public realm*.

Building Massing: The combination of volumetric shapes that make up a building’s form, such as bays, roofs, and the primary form of the building.

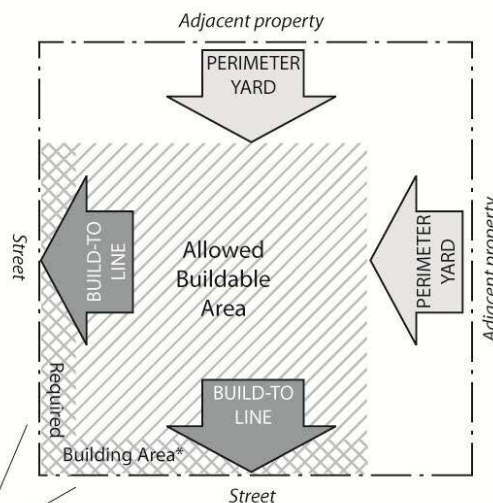
Build-to line: A line set in relation to the sidewalk area at the street fronting perimeter of a parcel to which a *building facade* must align for a certain proportion of the length of the build-to line along a parcels frontage. See Figure 7.a.

Lot Building Placement under
City of Tucson Land Use Code:
Perimeter Yards



No Required Building Area

Lot Building Placement under
Grant Road District:
Reduced Perimeter Yards and
Build-To Lines



Street Perimeter Yards in GRD = 0'

*Only a percentage of the required building area created by the Build-To Line must be occupied by a building. For specific percentages, see GRD Category Standards.

Figure 7.a: Build-to Line vs. Perimeter Yard diagram

5. Definitions



Figure 7.b: Clerestory windows.

Clerestory Windows: A high windows located above the eye level of a pedestrian that are arranged independently of windows or doors below the windows, and often high on an otherwise blank wall. See Figure 7.b.

Community Garden: A garden that is maintained by a group of individuals who eat the produce they grow or donate it. If the lot is occupied, row crops cannot be grown in the front yard.

Curved Corner Street Frontage: A particular type of *street frontage* where a large curb radius at an intersection causes a street property line to curve or take a non-90-degree angle. These occur both at street intersections and at the turnarounds that are associated with the Indirect Left Turns that will be constructed along Grant Road. See Figure 7.c.

Curved Street Frontage: A particular type of street frontage where the curving of Grant Road or other street rights-of-way creates a curved property line, see Figure 7.c.

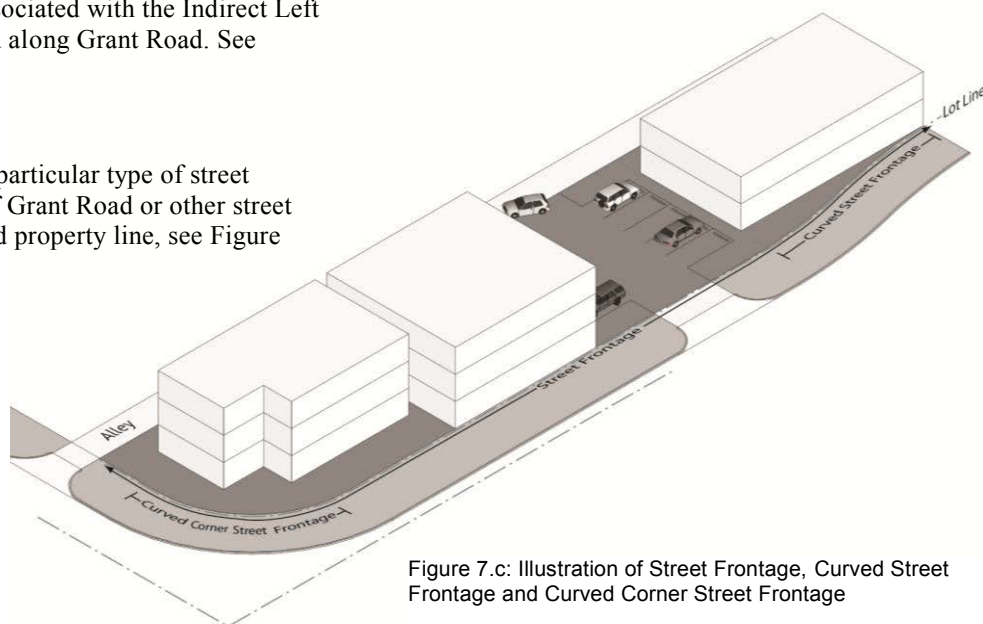


Figure 7.c: Illustration of Street Frontage, Curved Street Frontage and Curved Corner Street Frontage

District Parking: The provision of parking in either a surface lot or a structure that provides parking for multiple uses within the area, or “district”, surrounding the parking, allowing for shared parking and the reduction of the amount of parking provided on the individual properties for the uses within the district.

Encroachment: Encroachments are the allowance for private building elements or activities to extend into a *perimeter yard*, the public right-of-way or publicly accessible space. Encroachments include landscape structures such as arbors and canopies; building structures such as bays, eaves and awnings; and activities such as seating, dining and display of goods.

Functional Open Space: An exterior area that is for private or public passive or active recreational use. The GRD has both requirements for residential and commercial Functional Open Space which could include parks and plazas that are open to the public, terraces, courtyards, sports facilities, private recreation areas, playgrounds, swimming pools, community gardens, and passive sitting and gathering spaces; in relation to commercial space it may include dining and other exterior patron activities. Functional open space does not include required parking.

5. Definitions

Glazed Area: The transparent portion of windows (not including excluding skylights, light tubes and other roof fenestration) and doors.

Horizontal Mixed Use: The mixing of land uses within one development that distributes uses side-by-side and maximizes access to uses by creating a compact and walkable environment.

Human-Scale: Design which incorporates architectural and landscape elements that have a relationship to human proportions; that is, they are closely proportioned in size to human height, an arm's reach, or the grasp of a hand. See Figures 7.d through 7.g.

Individual Lot Frontage: The *street frontage* occupied by an individual parcel. If a development site is composed of multiple parcels, the individual lot frontage is the *street frontage* occupied by the development site. If the lot or development site fronts onto multiple streets, each street's frontage is treated individually except if the lot or site has uninterrupted *curved corner street frontage* in which case the two street frontages making up the corner are considered as one individual lot frontage. See Figure 7.c.



Figures 7.d and 7.e: Photo examples of buildings with human scaled articulation using more traditional stucco and wood construction.

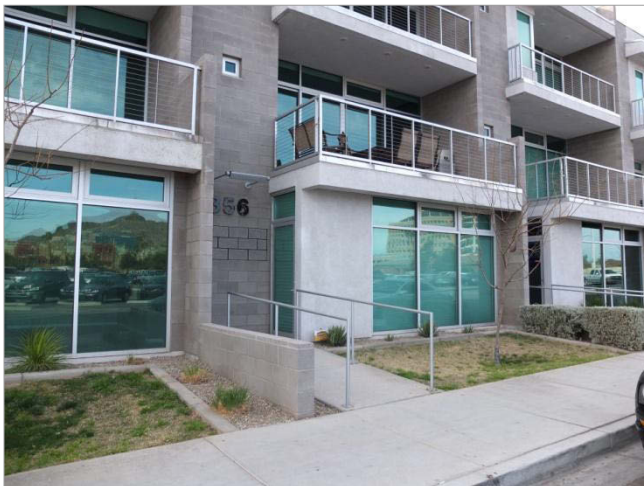


Figure 7.f and 7.g: and Photo examples of buildings with human scaled articulation using more modern steel and glass materials.

5. Definitions



Figure 7.h: Photo of a pedestrian pass-through between residences.

Pedestrian Pass-through: A path, private or public, that is accessible to pedestrians as a physical connection through a block between buildings, see Figure 7.h.

Perimeter Yard: An area defined by a minimum distance to separate buildings from adjacent property or streets. See Figure 7.a.

Public Frontage: The interface between lots and public streets as well as the interface between publicly accessible open spaces and what bounds them. The interface between lots and alleys are not public frontage.

Public Realm: Spaces and linkages that are owned and accessible to the public, and similar privately owned spaces that are open to the public at all times. Spaces and linkages may include streets, sidewalks, parks, plazas and pedestrian pass-throughs.

Semi-Public Realm: Spaces and linkages that are privately owned, but accessible to the public. Semi-public realm spaces may typically contain limitations such as accessible times. Semi-public realm spaces may include private streets, plazas and pedestrian pass-throughs.

Setback: For the purposes of the Grant Road District, a setback is the distance (minimum and/or maximum) from the *build-to line* to a *vertical element*, or building façade. Building façades that are set back (per setback allowance as defined in the frontage type) may be counted towards the % build-to line requirement.

Sidewalk Area: The area between a building site and the curb of a street used for pedestrian access and amenity. Sidewalk area is used for landscaping, either within a strip or tree well cutouts, a through area for pedestrian travel along the sidewalk, a “buffer” area along the edge of the sidewalk or adjacent building.

5. Definitions

Small Commercial Garden: A place where plants are grown for sale of the plants themselves or their products, and in which the plants or their products are sold at the lot where they are grown or off-site, or both, and in which no other items are sold. A small commercial garden can include orchards and vineyards.

Street Frontage: The interface between lots and public streets, which include Major and non-Major Streets, see Figure 7.c.

Transom Windows: The windows above a display window or door following the width and spacing of the windows and doors below, and separated from them by a transom bar or narrow portion of wall. See Figure 7.i.

Transparency: The building openings with no glass, clear glass or glass with low-e films, which allow 100% through visual connection between both sides of an opening.

Vehicular Use Area: Any area of a site or structure used for the parking, storage, or standing of motor vehicles. The vehicular use area includes access drives, maneuvering areas, refuse collection locations, loading spaces, and any landscaping and screening within ten (10) feet of these areas. (Ord. No. 10016, §2, 8/2/04)

Vertical Element: A landscape wall, seat wall, fence, arbor or porch.

Vertical Mixed Use: The mixing of land uses within one development that stacks multiple within one building, but compatible uses on the same parcel.

Visual Permeability: The ability of vertical surfaces to allow viewers to see through to the other side (e.g., windows and open fencing).



Figure 7.i: Transom window



5. Definitions

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Appendix A: Grant Road District Application Review Checklist

APPENDIX A: GRANT ROAD DISTRICT APPLICATION REVIEW CHECKLIST

Given the focus of the Grant Road District (GRD) on the relationship between buildings and uses on private property and adjacent streets and public open spaces, much of the review of applications will consider site and building design factors. Therefore many of the standards that a project must meet in order to be approved will need review by the City's Design Professional, in addition to those standards which will be reviewed by Planning & Development Services Department (PDS) staff and other staff that are part of the Community Design Review Committee (CDRC).

To facilitate review of proposed projects, the standards throughout the GRD zoning ordinance include a bullet that indicates who is meant to review each standard. Standards that are to be reviewed by PDS staff are indicated by a ► bullet, while standards meant to be reviewed by a City's Design Professional are indicated by a ❖ bullet. The following checklist uses similar indications and lists the groupings of standards in an outline of the GRD zoning ordinance.



Appendix A: Grant Road District Application Review Checklist

	Staff PDSD/CDRC Review	Design Professional Review
3. Grant Road District Standards		
<i>Standards</i>		❖ <input type="checkbox"/>
4. General Zoning Standards		
4.4 Block and Multi-Modal Circulation Standards		
4.4.3 Block Standards	➤ <input type="checkbox"/>	
4.4.4 Street Standards		
General Standards	➤ <input type="checkbox"/>	
<i>Alleys</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
<i>Sidewalk Areas</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
<i>Driveways</i>		❖ <input type="checkbox"/>
<i>Pedestrian Pass-throughs</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
<i>Water Harvesting in Streets</i>	➤ <input type="checkbox"/>	
4.4.5 Traffic Calming	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
4.5 Street Frontage and Building Placement Standards		
4.5.3 Perimeter Yard and Build-to Line (BTL)	➤ <input type="checkbox"/>	
4.5.4 Encroachments	➤ <input type="checkbox"/>	
4.5.5 Frontage Types		❖ <input type="checkbox"/>
4.6 Building Height, Massing, and Articulation Standards		
4.6.2 Massing and Articulation		
Massing		❖ <input type="checkbox"/>
Roofs		❖ <input type="checkbox"/>
Articulation		❖ <input type="checkbox"/>
Materials		❖ <input type="checkbox"/>
Entrances		❖ <input type="checkbox"/>
Windows		❖ <input type="checkbox"/>
Development Transition	➤ <input type="checkbox"/>	

Appendix A: Grant Road District Application Review Checklist

	Staff PDSD/CDRC Review	Design Professional Review
4.6.3 Privacy Considerations when Adjacent to Existing Residential Zoning		
4.6.3.2 Applicability	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
4.6.3.3 How to Use this Section		❖ <input type="checkbox"/>
4.6.3.4 Mitigation of Privacy Intrusions from Windows		❖ <input type="checkbox"/>
4.6.3.5 Mitigation of Privacy Intrusions from Balconies		❖ <input type="checkbox"/>
4.7 Site Design Standards		
4.7.1 Parking		
General Parking		
General Provisions	➤ <input type="checkbox"/>	
Required Number of Motor Vehicle Parking Spaces	➤ <input type="checkbox"/>	
Standards		
General Standards	➤ <input type="checkbox"/>	
Surface Parking Lot Design Standards		
Landscape		❖ <input type="checkbox"/>
Rainwater Runoff Management/Harvesting		❖ <input type="checkbox"/>
Screening	➤ <input type="checkbox"/>	
Pedestrian, Bicycle, and Vehicle Access and Circulation		❖ <input type="checkbox"/>
Lighting		❖ <input type="checkbox"/>
Frontage Types		❖ <input type="checkbox"/>
Shared Parking	➤ <input type="checkbox"/>	
District Parking		
District Parking Policies	➤ <input type="checkbox"/>	
District Parking Design Standards		❖ <input type="checkbox"/>
4.7.2 Bicycle Parking	➤ <input type="checkbox"/>	



Appendix A: Grant Road District Application Review Checklist

	Staff PDS/CDRC Review	Design Professional Review
4.7.3 Off-Street Loading and Utility Areas		
<i>General Provisions</i>	➤ <input type="checkbox"/>	
<i>Loading Design Standards</i>		
General Standards		❖ <input type="checkbox"/>
Location Requirements		❖ <input type="checkbox"/>
Access and Circulation Standards	➤ <input type="checkbox"/>	
Screening Requirements	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
Lighting Standards	➤ <input type="checkbox"/>	
Surfacing Standards	➤ <input type="checkbox"/>	
Performance Standards for Off-street Loading	➤ <input type="checkbox"/>	
<i>Utility Area Design Standards</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
<i>Solid Waste Facilities</i>	➤ <input type="checkbox"/>	
4.7.4 Landscape		❖ <input type="checkbox"/>
4.7.5 Screening		
<i>General Purpose Screening</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
<i>Screening at Interior Property Lines</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
4.7.6 Open Space		
<i>Public Open Space within Private Development</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
<i>Private and Common Open Space for Multiunit Residential</i>	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>
4.8 Lighting		
4.8.3 Standards		
<i>General Standards</i>	➤ <input type="checkbox"/>	
<i>Building Lighting</i>		❖ <input type="checkbox"/>
<i>Bicycle Parking</i>	➤ <input type="checkbox"/>	
4.9 Signs		
4.9.3 Standards		
<i>General Standards</i>	➤ <input type="checkbox"/>	
<i>Prohibited Signs</i>	➤ <input type="checkbox"/>	
<i>Signs By Tucson Sign Code District</i>	➤ <input type="checkbox"/>	

Appendix A: Grant Road District Application Review Checklist

	Staff PDSD/CDRC Review	Design Professional Review
4.10 Environmental Resources		
4.10.2 Standards		
<i>General Standards</i>	➤ <input type="checkbox"/>	
<i>Passive Solar and Cooling</i>		❖ <input type="checkbox"/>
<i>Stormwater Management, Water Conservation and Irrigation, and Rainwater Harvesting</i>		
Stormwater Management		❖ <input type="checkbox"/>
Water Conservation and Irrigation	➤ <input type="checkbox"/>	
Rainwater Harvesting	➤ <input type="checkbox"/>	❖ <input type="checkbox"/>